

Show Your Green! at the Chapman DeMary Trail Sunday, October 25 noon to 3 p.m.

You are invited to participate in the second annual Fall for the Trail and Paint Purcellville Green at the Chapman DeMary Trail on Sunday, October 25 from noon to 3 p.m. There are spaces for vendors and non-profits! This event features local businesses and organizations that focus on the environment or are environmentally friendly. It is co-hosted by The Nature Generation and the Town of Purcellville. We will have food trucks, live music, and activities for kids and families.



Vendor and non-profit spaces will be spread throughout the outdoor classroom/park area of the Chapman DeMary Trail and the parking lot above it. The fee for exhibitors is \$25 and non-profits are free. All exhibitors must bring all items required for their display, including tables, chairs, and tents or canopy with weights. To participate complete the form below and mail it with your check, payable to The Nature Generation, to the address on the form.

Contacts

- | | |
|--|---|
| ◇ Amie Ware, The Nature Generation
540-668-7738 aware@NatGen.org | ◇ Nancy Baker, Town of Purcellville Environmental Committee
540-533-7490 bizprocesssolutions@gmail.com |
| ◇ Anne Alba, Town of Purcellville Environmental Committee
571-239-4455 anne@solarodysseyinc.com | ◇ Melanie Scoggins, Town of Purcellville Event Specialist
540-751-2350 mscoggins@purcellvilleva.gov |

The Chapman DeMary Trail is a sustainable education partnership among The Nature Generation, Loudoun Valley High School, the Piedmont Environmental Council, and the Town of Purcellville

2015 Fall for the Trail & Paint Purcellville Green Vendor Registration Form

Name David Ward

Company Name Catoctin Creek Scenic River Advisory Committee

Address _____

City _____

State _____ Zip _____

Cell _____

Phone _____

Email _____

Web address _____

Description of business, what makes your business environmentally friendly, and what activity (if any) you will have during the event:

Circle the box next to your selection:

- ☐ Vendor: \$25
☒ Non-Profit: Free

Donations to NatGen are welcomed to support environmental education programs and enhance the habitat at the Chapman DeMary Trail.

You can register and pay by completing this form and mailing it with your check payable to
The Nature Generation to:

NatGen, ATTN: Fall for the Trail 2015
36903 Charles Town Pike | Purcellville, VA 20132

Registration fees will be divided between The Nature Generation for the Chapman DeMary Trail and the Town of Purcellville.

Catoctin Creek Scenic River Advisory Committee

Meeting Agenda Tuesday, October 6, 2015 4:00 – 6:00 PM

Location: Lovettsville Library, 12 North Light Street, Lovettsville, Virginia 20180


Committee: Anne Larson, Patti Psaris, Otto Gutenson, Norma Wilson, Eleanor Adams, Bruce Johnson, David Ward, David Nelson

Topics:


- Review Minutes
- Featherbed Lane John G. Lewis Memorial Bridge
- Development Projects
- 2015 Celebration of Loudoun's Historic and Environmental Heritage on December 5, 2015
- Nature Generation – Hail to the Trail, October 25, 2015
- Strategy and Mission Statement
- Announcements



Legend

 Catoclin Creek Scenic River

Conservation Easement

 Conservation Easement

Catoctin Creek Scenic River Advisory Committee

Meeting Minutes – September 21, 2015 (4:00 - 6:00 PM)

Committee Members: David Ward, David Nelson, Bruce Johnson, Otto Gutenson, Patti Psaris, Anne Larson, Eleanor Adams (Present); Norma Wilson (Absent)

Location: Lovettsville Library, 12 North Light Street, Lovettsville, VA

Call to order: 4:10 PM

Discussion

Review of Prior Meeting Minutes: (Draft minutes for August, Motion to approve with revised attendees list: Johnson, Second: Nelson, Voted: Unanimous.)

Goose Creek Scenic River Advisory Committee: Mark Peterson is stepping down as Chair. Anne Larson will contact Lynn Crump, DCR.

John G. Lewis Memorial Bridge on Featherbed Lane for:

- There was a third meeting on September 16 by VDOT. Preservation is still not being considered as an option. Virginia Department of Historic Resources (VDH) attended and is just now being engaged in the process. VDOT will forward three options for VDH to review. VDOT was asked to consider modifications to either eliminate the center pier or make it less noticeable.
- David Nelson will prepare and circulate a letter thanking VDOT and restating previous points.

Development Projects:

- Waterford Downs – No discussion.
- Gillen House on Creek Lane. The application was initiated in 2014 and constructed in 2015. The house is on 5+ acres parcel and the house located less than 200 feet from Catoctin Creek as the Scenic Creek Valley Buffer ordinance allows reduction to 100 feet is an approved forestry management plan is submitted. The parcel was noticed using County GIS where recent Occupancy Permits are shown.

Upcoming events: The committee voted to participate in the following two events. Both events are informational.

- Loudoun Preservation and Conservation Coalition “Festival” at Rehau in Leesburg on December 6 from 2 to 6 PM (Motioned by Nelson; Seconded by Psaris: Vote was unanimous).
- Nature Generation 2015 Fall for the Trail in Purcellville on Sunday October 25, noon to 3 PM. (Motioned by Psaris; Seconded by Nelson: Vote was unanimous).

Website: The work plan and budget was presented. While the concepts and strategy was aggregable, the cost were deemed too great and would significantly deplete the Catoctin Conservation Funds. The decision was to move forward using Committee volunteer resources. David Nelson will explore possible crowd sourcing for a logo.

Other Discussion: Catoctin Conservation Fund, administered by PEC. A meeting will be held in October to finalize the MOU. (Patti Psaris to contact Mike Kane)

Next meeting: TBD 10/5 or 10/6

Adjourn: 5:48 PM Motion: Johnson; Second: Nelson; Vote: Unanimous.

David Ward

From: Gdylg#Z dug#P gz dugC xgrxqz lhdw1frp A
Sent: Wkxwgd|/hshwp eh#57/#5348#-35#DP
To: *Sdw#Sdulv*#R wr #J xwhqvrq*#Hndgru#P #Dgdp v*#Dqgh#Ddwrg*#Euxfh#Mkqvrq#
+euxfh5<;C j p d1frp ,#Gdylg#D horq#gqghorq533<C j p d1frp ,
Subject: R yhu1ss1qj #dufho#dw#Hndwkuehg#Dqgh#Eulj h

I'll let you know what the Parcel Mapping Expert at Loudoun County comes up with.

From: Ward, David [mailto:David.Ward@loudoun.gov]
Sent: Thursday, September 24, 2015 8:45 AM
To: Rubis, Glen <Glen.Rubis@loudoun.gov>
Cc: info@loudounwatershedwatch.org
Subject: FW: How do I interpret overlapping parcels?

Doug is on it!

From: Ward, David
Sent: Thursday, September 24, 2015 8:35 AM
To: Packard, Doug
Subject: RE: How do I interpret overlapping parcels?

Thank you Doug!

From: Packard, Doug
Sent: Thursday, September 24, 2015 8:33 AM
To: Ward, David
Subject: RE: How do I interpret overlapping parcels?

Thanks David,
Unfortunately there is an error here. Even when a plat shows gores or gaps we shouldn't have one. The latest plat is not what is in the parcel layer for one of the items. It should display correctly tomorrow. I need to investigate to see what happened.
Thanks, Doug

From: Ward, David
Sent: Wednesday, September 23, 2015 4:19 PM
To: Packard, Doug
Subject: How do I interpret overlapping parcels?

Doug:

I noticed that there is an overlap between adjacent parcels. How do I interpret this?

299493233000 Waterford Downs with Plat 2000-0579
260254610000 Has no plat
261367996000 Waterford Downs Plat 2014-0281

This is the intersection where Featherbed Lane Bridge is located.



David Ward
[Water Resources Engineer](#)
Department of Building & Development

Loudoun County Government
1 Harrison St, MC #60
Leesburg, VA 20175
703-737-8670

Catoctin Creek Scenic River Advisory Committee

Meeting Minutes – August 10, 2015 (4:00 - 6:00 PM)

Committee Members: David Ward, David Nelson, Bruce Johnson, Otto Gutenson, Patti Psaris, Anne Larson, Eleanor Adams, (Present); Norma Wilson (Absent)

Location: Lovettsville Library, 12 North Light Street, Lovettsville, VA

Call to order: 4:21 PM

Discussion

Review of Prior Meeting Minutes: (Draft minutes for July, Motion to approve as is: Johnson, Second: Nelson, Voted: Unanimous.)

John G. Lewis Memorial Bridge on Featherbed Lane for:

- The naming ceremony was videoed by David Nelson.
- Two letters are to be written. One letter to VDHR noting the original road alignment, historic maps and recent LiDAR imagery. The second letter to VDOT regarding the July 23, 2015 meeting. Patti Psaris will draft and circulate for final delivery by August 24.

Priority Items: No discussion.

Development Projects: Waterford Downs – No discussion.

Website: Discussed possibility of using outside paid support for web site development. David Ward converted to using Weebly.com for web pages creation. David Nelson will explore means to develop a logo for the committee. David Nelson will prepare a section on history. The web site should include list of meetings and other events. The website should include current committee members.

Next meeting: September 21, 2015 (Revised after meeting due to Library conflict)

Adjourn: 6:00 PM Motion: Johnson; Second: Nelson; Vote: Unanimous.



David Ward <catoctinscenicriver@gmail.com>

Catoctin and Goose Creek Scenic River Advisory Committees - Joint Letter to BOS - Mapping Request - May 12, 2012

5 messages

David Ward <dward@loudounwireless.com>

Thu, Sep 24, 2015 at 6:15 AM

To: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, plantlover2@verizon.net, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>, benlawrence@verizon.net, pedaley@verizon.net, jgrokus@aol.com, jrisom3@aol.com, hecasey@msn.com, goosecreekriver@yahoo.com, Mark.Novak@loudoun.gov, surferjoetoo@yahoo.com, catoctinscenicriver@gmail.com
Cc: lynn.crump@dcr.virginia.gov

In a joint letter to Chairman York from the Goose and Catoctin Creek Scenic River Advisory Committees, dated May 12, 2012 (attached), the Committees presented three requests for mapping of the Scenic Rivers. To date there has been no response from the Chairman or County Staff, although there has been partial action taken.

The first request was to map and include both segments of the scenic rivers. A set of polygons representing the Scenic Rivers was added to the county on-line mapping webpage [WebLogis](#) and corresponding [metadata](#) posted. Action on this request is **complete**.

The second request was to add to the County Land Management Information System, LMIS, an identification of parcels adjacent to the Scenic Rivers. In 2012 we observed that a field (column in the parcel table) existed for "flagging" those parcels, however, there were no entries in the database for parcels. Accessing the underlying County GIS database, I have now found that this field has been populated with a first draft, but County Staff informs me that the work is not complete and needs to be expanded further than the 25-foot buffer query. Efforts by County Staff appear to have stopped as of October 2013. Action on this request is **partly complete**.

The third request was to map the [Zoning Ordinance](#) for the Scenic Creek Valley Buffer (Section 5-1000). Specific to the Scenic Rivers is to map the 200-ft buffer from the "scar line" of the Scenic Rivers. Further details of the ordinance may be found per attachment. To date there has been no action taken to define the "scar line" or map the scenic buffer. However, it should be noted that mapping the buffer has been a routine task undertaken by applicants and are required to be shown on development application documents. Staff reviews these maps in spite of the fact that there is no clear definition as to what a "scar line" is as this is a term unique to Loudoun County and not a common geomorphic term. The County continues to place the burden of mapping the Scenic Creek Valley Buffer on the development sector. Action on this request is **incomplete**.

I suggest that both Committees review the status of the request from 2012 and plan for follow-up actions.

- David Ward, Secretary

Catoctin Creek Scenic River Advisory Committee

Creating a community of stewardship for the Catoctin Creek

CatoctinScenicRiver@gmail.com

www.CatoctinScenicRiver.org

Watershed address: Milltown Creek, Catoctin Creek, Potomac, Chesapeake Bay

2 attachments



Letter_BOS_mapping_request_May_14_2012_resize.pdf

386K



Pages from 04 - Revised '93 Zoning Ordinance_20150714_Section_5-1000_ScenicCreekValleyBuffer.pdf

107K

Benjamin Lawrence <benlawrence@verizon.net>

Thu, Sep 24, 2015 at 11:22 AM

To: David Ward <dward@loudounwireless.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, plantlover2@verizon.net, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>, Phil Daley <pedaley@verizon.net>, Jone Rokus <jgrokus@aol.com>, John Isom <jrisom3@aol.com>, hecasey@msn.com, Mark Peterson <goosecreekriver@yahoo.com>, Mark.Novak@loudoun.gov, surferjoetoo@yahoo.com, catoctin scenicriver@gmail.com, Lynn Crump <lynn.crump@dcr.virginia.gov>

I agree David. LC has all ways needed a push.



The Lawrence Group

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•
***Box 12
Leesburg Va. 20178-0012
703 777 5777p - 703 728 5777c
benlawrence@verizon.net***

On Sep 24, 2015, at 06:15, David Ward <dward@loudounwireless.com> wrote:

a joint letter to Chairman York from the Goose and Catoctin Creek Scenic River Advisory Committees, dated May 12, 2012 (attached), the Committees presented three requests for mapping of the Scenic Rivers. To date there has been no response from the Chairman or County Staff, although there has been partial action taken.

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200-ft buffer from the “scar line” of the Scenic Rivers. Further details of the ordinance may be found per attachment. To date there has been no action taken to define the “scar line” or map the scenic buffer. However, it should be noted that mapping the buffer has been a routine task undertaken by applicants and are required to be shown on development application documents. Staff reviews these maps in spite of the fact that there is no clear definition as to what a “scar line” is as this is a term unique to Loudoun County and not a common geomorphic term. The County continues to place the burden of mapping the Scenic Creek Valley Buffer on the development sector. Action on this request is **incomplete**.

I suggest that both Committees review the status of the request from 2012 and plan for follow-up actions.

Mr. Blake Netherwood <surferjoetoo@yahoo.com>

Thu, Sep 24, 2015 at 1:37 PM

Reply-To: "Mr. Blake Netherwood" <surferjoetoo@yahoo.com>

To: Benjamin Lawrence <benlawrence@verizon.net>, David Ward <dward@loudounwireless.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, "plantlover2@verizon.net" <plantlover2@verizon.net>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>, Phil Daley <pedaley@verizon.net>, Jone Rokus <jgrokus@aol.com>, John Isom <jrisom3@aol.com>, "hecasey@msn.com" <hecasey@msn.com>, Mark Peterson <goosecreekriver@yahoo.com>, "Mark.Novak@loudoun.gov" <Mark.Novak@loudoun.gov>, "catoctin scenicriver@gmail.com" <catoctin scenicriver@gmail.com>, Lynn Crump <lynn.crump@dcr.virginia.gov>

David & Ben... Thank you for sharing the attachments, insights, and follow-up suggestions. As a relative newby to GCSRAC, I learned from the attachments, including the well-crafted May 2012 email history. It seems there has been no reply or further followup, but I may be mistaken.

I had little familiarity with the cited Revised '93 Zoning Ordinance 20150714 Section 5-5100 Scenic Creek Valley Buffer, so I let my Google fingers do the walking and learned more, including recent LoCo Planning Commission Summary Agenda of Jul 21, 2015 at <http://www.loudoun.gov/ArchiveCenter/ViewFile/Item/4700>

and May 5, 2015 at http://loudoun.granicus.com/MetaViewer.php?view_id=63&clip_id=4116&meta_id=85217

Also 1999 revision to 5-5100 at <http://www.loudoun.gov/DocumentCenter/View/17533>

I regret missing the Sep. 15 GCSRAC meeting and I hear Mark Peterson may be stepping down as Chairperson. Please advise if there is a planned followup meeting.

I've been approached to consider nomination for the Chair if/when it is vacant. I'm flattered with this consideration; however, I'm not best. I fully support GCSRAC's importance, but lack the history essential to this position's seasoned insight and potential effectiveness.

V/r...Blake Netherwood

[Quoted text hidden]

David Ward <dward@loudounwireless.com>

Thu, Sep 24, 2015 at 5:13 PM

To: "Mr. Blake Netherwood" <surferjoetoo@yahoo.com>, Benjamin Lawrence <benlawrence@verizon.net>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, plantlover2@verizon.net, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>, Phil Daley <pedaley@verizon.net>, Jone Rokus <jgokus@aol.com>, John Isom <jrisom3@aol.com>, hecasey@msn.com, Mark Peterson <goosecreekriver@yahoo.com>, Mark.Novak@loudoun.gov, catoctin scenicriver@gmail.com, Lynn Crump <lynn.crump@dcr.virginia.gov>

Blake:

Note that your reference URL to 5-1000 is outdated and the 2 pages I originally attached were revised 2007.

This spring/summer/fall there are two significant zoning changes in motion. One is the revision to the Steep Slopes Ordinance. The other is revisions to Floodplain Overlay District Ordinance. These changes are the outcome of the Zoning Ordinance Action Group (ZOAG). The committee is primarily composed of developers and engineers seeking to revise the ordinances to be more "business friendly". There are numerous meetings and draft revisions that would require hours to explore in detail. You might be interested to review the various posts at <http://www.loudouncoalition.org/> While there has been public input at various stages, the Floodplain Zoning changes go to Board of Supervisors on October 14th and the Steep Slopes on October 16th.

Go to the hyperlink to see letters from the Coalition and quick access to County reports. Both ordinance potentially affect the "scenic" nature of both creeks, hence please take some time to review and engage as appropriate.

There is another action underway that should be of interest to both Scenic River Committees. This is revised floodplain maps. The maps and the changes are published in an interactive map. The appeals process has been published in the local newspaper for those who wish to file an appeal by December 22, 2015.

Additionally, the Water Resources Technical Advisory Committee is meeting on Monday September 28th to further discuss completing the Phase II evaluation of funding options for a comprehensive watershed management program. Send me an email (david.ward@loudoun.gov) for more information. These are open public meetings at the Loudoun County Government Center.

Also there is a **Photo Contest** underway as part of **Family Stream Day** on October 18th.

- David Ward, Secretary

Catoctin Creek Scenic River Advisory Committee

Creating a community of stewardship for the Catoctin Creek

CatoctinScenicRiver@gmail.com

www.CatoctinScenicRiver.org

<https://www.facebook.com/Catoctin-Creek-Scenic-River-993277240694342/timeline/>

Watershed address: Milltown Creek, Catoctin Creek, Potomac, Chesapeake Bay

From: Mr. Blake Netherwood [mailto:surferjoetoo@yahoo.com]

Sent: Thursday, September 24, 2015 1:38 PM

To: Benjamin Lawrence <benlawrence@verizon.net>; David Ward <dward@loudounwireless.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>; Otto Gutenson <otto100@loudounwireless.com>; Eleanor M Adams <emcgadams@earthlink.net>; Anne Larson <annelarson2006@gmail.com>; plantlover2@verizon.net; Bruce Johnson <bruce298@gmail.com>; David Nelson <danelson2009@gmail.com>; Phil Daley <pedaley@verizon.net>; Jone Rokus <jgrokus@aol.com>; John Isom <jrisom3@aol.com>; hecasey@msn.com; Mark Peterson <goosecreekriver@yahoo.com>; Mark.Novak@loudoun.gov; catoctinscenicriver@gmail.com; Lynn Crump <lynn.crump@dcr.virginia.gov>

Subject: Re: Catoctin and Goose Creek Scenic River Advisory Committees - Joint Letter to BOS - Mapping Request - May 12, 2012

[Quoted text hidden]

Mr. Blake Netherwood <surferjoetoo@yahoo.com>

Thu, Sep 24, 2015 at 6:17 PM

Reply-To: "Mr. Blake Netherwood" <surferjoetoo@yahoo.com>

To: David Ward <dward@loudounwireless.com>, Benjamin Lawrence <benlawrence@verizon.net>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, "plantlover2@verizon.net" <plantlover2@verizon.net>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>, Phil Daley <pedaley@verizon.net>, Jone Rokus <jgrokus@aol.com>, John Isom <jrisom3@aol.com>, "hecasey@msn.com" <hecasey@msn.com>, Mark Peterson <goosecreekriver@yahoo.com>, "Mark.Novak@loudoun.gov" <Mark.Novak@loudoun.gov>, "catoctinscenicriver@gmail.com" <catoctinscenicriver@gmail.com>, Lynn Crump <lynn.crump@dcr.virginia.gov>

David...Thanks so much for your cornucopia of info and links. However, I'm reminded of, "Be careful what you ask for (or infer)."

As a GCSRAC volunteer, I try to limit time I can dedicate pro bono to our fine county without going down the slippery slope of hip-deep research and concomitant follow-on and public interest expectations. That's my verbose way of saying there are folks among our BoS and Loudoun Planning Dept. that are far more on top of zoning issues than I could ever hope to be with keeping developers, engineers, and other commercial stakeholders at bay with their self-serving interests. That said, I'll still stay tuned and remain informed and participate as I can on such matters as a stakeholder citizen. Cheers...Blake

[Quoted text hidden]



David Ward <catoctin scenicriver@gmail.com>

Catoctin Creek Scenic River Advisory Committee - Participation in Dec. 6 Loudoun Coalition Festival

3 messages

David Ward <dward@loudounwireless.com>

Tue, Sep 22, 2015 at 5:58 AM

To: avanhuyck@aol.com

Cc: catoctin scenicriver@gmail.com, dave w sward@gmail.com, Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>

AI:

Catoctin Creek Scenic River Advisory Committee would be pleased to participate in the *2015 Celebration of Loudoun's Historic and Environmental Heritage* on December 5, 2015. We understand that a contribution of \$25 will be requested from each group to cover refreshments, publicity, and other expenses. The contribution will be due on November 16, 2015. Information on how to address the check and where to send it will be provided later.

- David Ward, Secretary

Catoctin Creek Scenic River Advisory Committee

Creating a community of stewardship for the Catoctin Creek

CatoctinScenicRiver@gmail.com

www.CatoctinScenicRiver.org

Watershed address: Milltown Creek, Catoctin Creek, Potomac, Chesapeake Bay



INVITATION_TO_REHAU_DEC6_EVENT_Catoctin_Creek_Scenic_River_Advisotry_Committee.pdf

559K

David Ward <catoctinscenicriver@gmail.com>

Tue, Sep 22, 2015 at 9:26 AM

To: David Ward <dward@loudounwireless.com>

Cc: Al Van Huyck <avanhuyck@aol.com>, David Ward <davewsward@gmail.com>, Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>

Correction, Date should be Sunday December 6th, not Saturday the 5th.

[Quoted text hidden]

Al Van Huyck <al.vanhuyck@gmail.com>

Wed, Sep 23, 2015 at 10:24 AM

To: David Ward <catoctinscenicriver@gmail.com>

Cc: David Ward <dward@loudounwireless.com>, David Ward <davewsward@gmail.com>, Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>

Hi David: Great news. We will get back to you with the details. Al

[Quoted text hidden]



David Ward <catoctin scenicriver@gmail.com>

Catoctin Creek Scenic River Advisory Committee - Participation in October 25th "Fall for the Trail and Paint Purcellville Green "

2 messages

David Ward <dward@loudounwireless.com>

Tue, Sep 22, 2015 at 6:33 AM

To: aware@natgen.org

Cc: catoctin scenicriver@gmail.com, davewsward@gmail.com, Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>

Amie:

Catoctin Creek Scenic River Advisory Committee would be pleased to participate in the second annual *Fall for the Trail and Paint Purcellville Green* at the Chapman DeMary Trail on Sunday, October 25 from noon to 3 p.m. We understand that all exhibitors must bring all items required for their display, including tables, chairs, and tents or canopy with weights. As a non-profit there is no fee.

- David Ward, Secretary

Catoctin Creek Scenic River Advisory Committee

Creating a community of stewardship for the Catoctin Creek

CatoctinScenicRiver@gmail.com

www.CatoctinScenicRiver.org

Watershed address: Milltown Creek, Catoctin Creek, Potomac, Chesapeake Bay



2015 Fall for the Trail and Paint Purcellville Green Vendor Registration Form_Catoctin_Creek_Scenic_River.pdf

442K

Amie Ware <aware@natgen.org>

Tue, Sep 22, 2015 at 9:36 AM

Reply-To: aware@natgen.org

To: David Ward <dward@loudounwireless.com>

Cc: catoctin scenicriver@gmail.com, davevsward@gmail.com, Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>

David--Hi there. This is fabulous! We will add you to this list of participants and look forward to seeing you at the Chapman DeMary Trail on Sunday October 25!

All the best,
Amie

Amie Ware

Teach Green Program Director

The Nature Generation

540-668-7738/703-298-8832 (cell)

aware@natgen.org

www.NatGen.org

Find us on Facebook:

www.facebook.com/thenaturegeneration

Follow us on Twitter: @TheNatGen

Proud member of:

Combined Federal Campaign of the National Capital Area #37682

United Way Campaign of the National Capital Area #9845

2011 Commonwealth of Virginia Campaign #8433

From: David Ward [mailto:dward@loudounwireless.com]

Sent: Tuesday, September 22, 2015 6:33 AM

To: aware@natgen.org

Cc: catoctin scenicriver@gmail.com; davevsward@gmail.com; 'Patti Psaris'; 'Otto Gutenson'; 'Eleanor M Adams'; 'Anne Larson'; Bruce Johnson; David Nelson

Subject: Catoctin Creek Scenic River Advisory Committee - Participation in October 25th "Fall for the Trail and Paint Purcellville Green "

[Quoted text hidden]



David Ward <catoctinscenicriver@gmail.com>

Catoctin Creek Scenic River Advisory Committee - Updated parcel mapping4 messages

David Ward <dward@loudounwireless.com>

Thu, Sep 24, 2015 at 6:43 AM

To: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>

Cc: davevsward@gmail.com, catoctinscenicriver@gmail.com

In accessing the County GIS, I found that the table of parcels now "first draft" data in the field which is used to flag "Scenic River". I say "first draft" as a 25-ft buffer was performed by County Staff in 2013, but this was deemed to be inadequate. Further efforts by County staff were halted in 2013 without completion of the task which we requested in May 2012.

I have also found that certain segments of the creek have "voids" in the parcel layer. In years past I don't recall seeing any voids in Catoctin, just Goose Creek. As an example, I've attached a map of Taylorstown showing that Nelson owns two parcels, separated by a parcel (closely conforming to the creek which has no ownership. I guess one might consider this truly "Waters of the US". Also note that this map includes 4-inch resolution imagery from spring 2015 which is not available online.

I further note that the John G. Lewis Memorial Bridge on Featherbed Lane falls into two parcels and have submitted a request to Loudoun County Office of Mapping to review and explain. It is not normal for two polygons to overlap and obviously reflect the problems associated with generating consistent topology from surveyor land records. I'll be following up on this later.

- David Ward, Secretary

Catoctin Creek Scenic River Advisory Committee

Creating a community of stewardship for the Catoctin Creek

CatoctinScenicRiver@gmail.com

www.CatoctinScenicRiver.org

Watershed address: Milltown Creek, Catoctin Creek, Potomac, Chesapeake Bay

**Catoctin_Scenic_River_Sept_2015_Taylorstown.pdf**

1882K

David Nelson <danelson2009@gmail.com>

Thu, Sep 24, 2015 at 6:59 AM

To: David Ward <dward@loudounwireless.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, davewsward@gmail.com, catoctin scenicriver@gmail.com

Thanks Dave

Can you send me the Lidar for taylorstown historic area?

Including the winery behind the store?

Sincerely,

David Nelson

[703-989-8072](tel:703-989-8072)

[Quoted text hidden]

| <Catoctin_Scenic_River_Sept_2015_Taylorstown.pdf>

David Ward <dward@loudounwireless.com>

Thu, Sep 24, 2015 at 7:03 AM

To: David Nelson <danelson2009@gmail.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, davewsward@gmail.com, catoctin scenicriver@gmail.com

I will prepare a detailed topography map, though covering the entire Taylorstown historic district will not show all the details which is contained in 2012 LiDAR. I'll explore creating a map of just the winery. These maps need to be done at the County. I'm on vacation today.

- David Ward

From: David Nelson [mailto:danelson2009@gmail.com]

Sent: Thursday, September 24, 2015 6:59 AM

To: David Ward <dward@loudounwireless.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>; Otto Gutenson <otto100@loudounwireless.com>; Eleanor M Adams <emcgadams@earthlink.net>; Anne Larson <annelarson2006@gmail.com>; Bruce Johnson <bruce298@gmail.com>; davewsward@gmail.com; catoctinscenicriver@gmail.com

Subject: Re: Catoctin Creek Scenic River Advisory Committee - Updated parcel mapping

[Quoted text hidden]

David Nelson <danelson2009@gmail.com>

Thu, Sep 24, 2015 at 7:14 AM

To: David Ward <dward@loudounwireless.com>

Cc: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, Bruce Johnson <bruce298@gmail.com>, davewsward@gmail.com, catoctinscenicriver@gmail.com

Also I have a basic history of the creek document first draft that I will send out shortly for review. We can use this as source material for the website and other future documents.

I'm working on some other options to help us get help for you to anchor the site. I really would like to get this done before dec 6. Let's discuss in the next few days.

Sincerely,

David Nelson
[703-989-8072](tel:703-989-8072)

[Quoted text hidden]



David Ward <catoctinscenicriver@gmail.com>

FW: Summary of the September 2015 Meeting

1 message

David Ward <dward@loudounwireless.com>

Sun, Oct 4, 2015 at 9:52 AM

To: Patti Psaris <pattipsaris@rstarmail.com>, Otto Gutenon <otto100@loudounwireless.com>, Eleanor M Adams <emcgadams@earthlink.net>, Anne Larson <annelarson2006@gmail.com>, plantlover2@verizon.net, Bruce Johnson <bruce298@gmail.com>, David Nelson <danelson2009@gmail.com>, Alice Power <powergang@aol.com>

Cc: David Ward <catoctinscenicriver@gmail.com>

From: Loudoun County Preservation and Conservation Coalition [mailto:mp=loudouncoalition.org@mail139.suw12.mcsv.net] **On Behalf Of** Loudoun County Preservation and Conservation Coalition
Sent: Sunday, October 4, 2015 9:42 AM
To: David <dward@earthwardconsulting.com>
Subject: Summary of the September 2015 Meeting

[Summary of the September 29, 2015, meeting](#)

[View this email in your browser](#)





September Meeting Summary

loudouncoalition.org

4 p.m. September 29, 2015
Purcellville Train Station

1. **PRESENT:** Al Van Huyck (Chair), Bob Shuey (Vice Chair Heritage), Ed Gill, Ard Geller, Mitch Diamond, Mike Kane, Phil Ehrenkranz, Maura Copeland, Margret Good, Brad Bradshaw, Alysoun Mahoney, Su Webb, Gem Gingol, Patty Rogers-Renner, and Martha Polkey. Guest: Patricia Rostkowski.
2. Mitch Diamond reported that the Rural Roads Committee will hold a workshop by invitation on November 20, 2015, to explore ways to educate the public on the historic importance of unpaved rural roads.
3. Al Van Huyck said that the rural roads mapping project held a meeting with Larry Camp of VDOT and the County GIS staff. It turned out the VDOT has just gone on line in June with a very similar system for taking in information both directly on the website and by phone on problems with all roads in Virginia. The citizen gets an email response as

acknowledgement. The VDOT map is not available for public viewing. Van Huyck suggested that a future meeting be held to explore if the rural road information from VDOT could be automatically downloaded onto the County rural road map and if Loudoun citizens can use our app to upload onto the VDOT system. Then the Rural Roads Committee could undertake a public education initiative to encourage widespread use of the combined systems.

4. Al Van Huyck noted that the Steep Slope Zoning Amendments will be discussed with county staff, ZOAG, and the Coalition at a meeting scheduled for Monday, October 5, 2015. Then the issues will be considered by TLUC at their October 16, 2015 at 9:00 AM meeting and probably go to the BOS for action in November. A updated note on objections will be circulated to the membership prior to the TLUC meeting.
5. Al Van Huyck reported that the BOS public hearing on the proposed Flood Plain amendments is scheduled for October 14, 2015 at 6:00 PM in the County Office Building. It is hoped that at least six or seven Coalition member groups will appear to oppose certain amendments. An updated report will be circulated prior to the public hearing. Alysoun Mahoney said that LWC is preparing an action alert for their members on the impact of the proposed Flood Plain amendments on wildlife. Gem Bingol said PEC is also putting together an action alert.
6. Mike Kane reported for the Conservation Easement Committee. The expanded membership of the committee now includes: Ed Gill, Margret Good, Al Van Huyck, Wendy Beebe, and Mike Kane as Chair. Paul Ziluca is also available via email for consultation. A meeting date with Charles Yudd has been set for October 27, 2015 from 1-3 PM to review the memo we submitted on out-sourcing the stewardship and monitoring functions and to begin a long-term focus on the development easements held by the County. A preparatory meeting will be held in Mike Kane's office in Middleburg at 1 p.m. on Monday, October 19, 2015, to brief new members of the committee and to prepare for the meeting with Charles Yudd and County staff.
7. David Nelson updated the Coalition on the John G. Lewis bridge. Another meeting was held with VDOT on September 16, 2015. VDOT noted the archeological study was completed with the finding of one Indian artifact and a conclusion that the options being considered will not affect the viewshed. There was discussion of the options, but none truly meet the expectations of the community. The representative of the Virginia Department of Historic Resources was present and pointed out that they had not yet been contacted to perform their mandatory review of the options to assess their impact on the bridge and its historic designation. The Coalition members were urged to share their comments with the VDHS and we will await their report.
8. Al Van Huyck suggested a Coalition committee role in an effort to get reconsideration of

using performance standards to best adopt “farm breweries” into the rural Loudoun landscape. A committee was formed with Al Van Huyck, Mitch Diamond, Maura Copeland. The first step will be to meet with the Janet Clarke and staff to assess the present situation. The objective is to be ready with our position when the ZOAG rural uses report moves forward. This is likely to be after the November 3, 2015 election.

9. Al Van Huyck and Patty Rogers-Renner reported on the Coalition Festival scheduled for Sunday, December 6, 2015 at REHAU, Leesburg. Pat Daly, CEO of the George C. Marshall Center, has agreed to print posters and program, etc., for the event as well as lend us table clothes for the table displays. The Loudoun Preservation Society has agreed to hold the funds for the Festival, which is much appreciated. The main task is to select an interesting keynote speaker; several candidates were discussed. At the October meeting the Festival Committee will ask for volunteers to take on the multiple tasks needed on the day of the event. ***It was stressed that all member groups get in their commitment form for the Festival as soon as possible, but not later than our Coalition meeting on October 27, 2015.***
10. Phil Ehrenkranz, editor of the In Our Backyard Column, reported that he is working with four authors on articles in final revision. ***New articles from members are welcome. The instructions for submitting an article was distributed to all members.***
11. Al Van Huyck noted that there are pending amendments to the Noise Regulations in Loudoun. We need to familiarize ourselves with the proposals to insure there is no effort to weaken current standards. The Planning Commission public hearing is scheduled for November 17, 2015.
12. Patricia Rostkowski spoke of her concern with encouraging Dark Sky in Loudoun. She distributed some printed materials from the International Dark-Sky Association. Alysoun Mahoney commented that LWC was concerned with the issue from the impact on wildlife. Al Van Huyck recalled that Joe Coleman prepared materials in 2008 to encourage the adoption of a Dark Sky Ordinance. The recent PEC newsletter contained an article on the issue. There was interest in the subject as a potential 2016 activity. Alysoun Mahoney and Patricia Rostkowski would meet with any interested Coalition members to consider the subject further and report at a future meeting of the Coaliton.
13. Al Van Huyck said that it is time to schedule another meeting with Tim Hemstreet, County Administrator, in early November.
14. Alysoun Mahoney briefed the Coaliton on the LWC/VDOT/Dominion Power initiative to create life-sustaining refuges for the Monarch Butterfly and other pollinators. The effort will include a mass plant of 8,000 pollinator friendly plants.
15. The main topic of the October meeting was to be the draft Strategic Plan circulated by email to the members. Ard Geller stated that he thought the draft Strategic Plan was not

well organized or complete. However, there was not time to discuss it and it was decided that a special meeting would be scheduled in October for those members who are interested to work on the 2016-2020 Strategic Plan.

16. There did appear to be agreement that the Coalition should establish a committee to act as a Stakeholder and monitor the review of the General Plan proposed for 2016.

The next meeting of the Coalition will be held at 4 p.m. on Tuesday, October 27, 2015, at the Purcellville Train Station.

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Contact us at:

mp@loudouncoalition.org

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This email was sent to dward@earthwardconsulting.com

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Loudoun County Preservation and Conservation Coalition · PO Box 368 · Roundhill, VA 20142 · USA

The MailChimp logo, featuring the word "MailChimp" in a white, cursive script font, centered within a dark gray rounded rectangle.



David Ward <davewward@gmail.com>

Fwd: Comments on Featherbed Ln - John G Lewis Bridge

1 message

David Nelson <danelson2009@gmail.com>

Fri, Sep 25, 2015 at 4:23 PM

To: Dave Ward <davewward@gmail.com>, Patti Psaris <pattipsaris@rstarmail.com>, Eleanore Adams <emcgadams@earthlink.net>, Bruce Johnson <bruce298@gmail.com>, Anne Larson <annelarson2006@gmail.com>, Otto Gutenson <otto100@loudounwireless.com>, Norma Wilson <plantlover2@verizon.net>

I just realized that you all may not have seen this letter from PEC.

Dan Holmes is Dir of State Policy.

----- Forwarded message -----

From: **Julie Bolthouse** <jbolthouse@pecva.org>

Date: Mon, Aug 31, 2015 at 8:59 AM

Subject: Fwd: Comments on Featherbed Ln - John G Lewis Bridge

To: Nathan Holth <nathan@historicbridges.org>, Mitch / Lucy <mdiamond9@earthlink.net>, David Nelson <danelson2009@gmail.com>

Hello everyone,
Here are the comments PEC submitted regarding Featherbed Lane Bridge.
Julie

Julie Bolthouse
Fauquier County Field Officer

Piedmont Environmental Council
www.pecva.org
96 Main St.
Warrenton, VA 20186
[540-347-2334](tel:540-347-2334) ext. 7042

"Here is your country. Cherish these natural wonders, cherish the natural resources, cherish the history and romance as a sacred heritage, for your children and your children's children. Do not let selfish men or greedy interests skin your country of its beauty, its riches or its romance." ~Theodore Roosevelt

----- Forwarded message -----

From: **Dan Holmes** <dholmes@pecva.org>

Date: Wed, Aug 26, 2015 at 12:44 PM

Subject: Comments on Featherbed Ln - John G Lewis Bridge

To: "Ezell, Raymond" <Raymond.Ezell@vdot.virginia.gov>

Cc: Julie Bolthouse <jbolthouse@pecva.org>

Dear Mr. Ezell,

Please find attached comments on the Featherbed Ln./John G. Lewis Bridge project.

Best,

Dan

Dan Holmes
Director of State Policy
Piedmont Environmental Council
(571) 213-4250

--

Regards,

David Nelson
[703-989-8072](tel:703-989-8072)



JohnGLewisBridgeComments.pdf
194K



You Are Invited to Participate in the
2015 Celebration of Loudoun's Historic and Environmental Heritage
sponsored by the
Loudoun County Preservation and Conservation Coalition
Sunday, December 6, 2015, 2 to 6 p.m.
Hosted at

REHAU, 1501 EDWARDS FERRY ROAD N.E., LEESBURG

Once again the members of the Loudoun County Preservation and Conservation Coalition (LCPCC) are organizing a Festival to introduce each of our member groups to the newly elected Board of Supervisors, the Loudoun State Legislative representatives, and the general public.

As before, all members of the LCPCC are invited to bring their information and displays of their activities to the event where a six foot table will be provided.

A contribution of \$25 will be requested from each group to cover refreshments, publicity, and other expenses. The contribution will be due on November 16, 2015. Information on how to address the check and where to send it will be provided later.

Please email or mail the following information to:

Al Van Huyck, P.O. Box 226, Round Hill, VA 20142 or email to avanhuyck@aol.com

_____x_____ **Yes, we will participate** _____ **No, we cannot participate**

NAME OF THE ORGANIZATION: # # o k ° # _____

MAILING ADDRESS: _38659 Bolington Rd, Lovettsville, VA 20180_____

NAME OF CONTACT PERSON: __David Ward_____

PHONE: _540-822-5092_____ **EMAIL:** _catoctinscenicriver@gmail.com_____



The Goose Creek Scenic River Advisory Committee

May 14, 2012

Scott K. York, Chairman
Loudoun County Board of Supervisors
P.O. Box 7000
Mailstop #01
Leesburg, VA 20177-7000

Subject: Mapping Requests in support of Catoctin Creek and Goose Creek Scenic River Advisory Committees

Loudoun County is fortunate to have two designated scenic rivers flowing through our area, Goose Creek and Catoctin Creek. Each of these has a state-appointed advisory committee associated with assisting in maintaining the character of those valley corridors. The Catoctin Creek and Goose Creek Scenic River Advisory Committees encourage the Loudoun County Government to implement the following actions. These actions will serve to raise the awareness of our citizens and aid County staff in the performance of their work. One of the Committees' duties is to advise the local government and the Director of the Department of Conservation and Recreation in administering that section of designated Scenic River within the local government's jurisdiction (§ 10.1-406.1). We believe these recommendations will be of benefit to the County, the Scenic River Committees and those involved in the Scenic River Program. To that end, we have copied Supervisors Higgins, Clarke, and Buona whose districts abut or encompass these designated scenic rivers.

Both scenic river advisory committees were established by the Virginia Scenic Rivers Program. The Program's intent is to identify, designate and help protect rivers and streams that possess outstanding scenic, recreational, historic and natural characteristics of statewide significance for future generations.

Requested Actions:

- 1) **Map the Scenic Catoctin Creek and Goose Creek.** We encourage the County to include the state-designated scenic portion of both rivers on maps, include this as a layer on the on-line mapping system, and make digital data available to the public. The Catoctin Creek from bank to bank in Loudoun County from the Town of Waterford to its junction with the Potomac River, a distance of approximately 16 river miles, is designated a component of the Virginia Scenic Rivers System. (§ 10.1-410). The Department of Conservation and Recreation provides a map at http://www.dcr.virginia.gov/recreational_planning/srmain.shtml.

The Goose Creek is a designated scenic river, beginning at its headwaters in Fauquier County and flowing more than 40 miles to its junction with the Potomac River. The Goose Creek is also an important drinking water source for parts of the Northern Virginia Region.

- 2) **Implement the land records system to identify parcels adjacent to the scenic river.** The County's Land Management Information System (LMIS) is a relational database that already includes fields for each parcel throughout the County including: voting district, watershed, floodplain and scenic river. We have found that the field for the scenic river flag is completely blank and has never been implemented. We understand that the relational database is



The Goose Creek Scenic River Advisory Committee

maintained and updated daily using the GIS to overlay and update these fields through an automated process. The County should create the scenic river buffer polygon and add this to the automated update process. By populating the field for Scenic River, County staff will know that development applications on a parcel are impacting the Scenic River Corridor. Once the buffer polygons are developed, the updating would be automated along with other overlays with no additional effort.

- 3) **Map the Zoning ordinance for Scenic Creek Valley Buffer.** While most zoning districts are mapped in the County's GIS, there is no GIS layer to map the Scenic Creek Valley Buffer per zoning ordinance (Section 5-1000) of the 1993 Loudoun County Zoning Ordinance. We are specifically interested in having the County map the 200-ft buffer from the scar line of the Scenic Creek. We understand that development applications are required to show the extent of the Scenic Creek Valley Buffer on site plans and that the County staff must evaluate each development application to ensure compliance. Our suggestion would facilitate application review if the boundary was mapped and the GIS files made available to the public.

We look forward to County action on the above requests and are available if we can be of assistance in any way.

Respectfully submitted,

Patti Psaris, Chairman
Catoctin Scenic River Advisory Committee
catoctin scenicriver@gmail.com

Mark Peterson, Chairman
Goose Creek Scenic River Advisory Committee
goosecreekriver@yahoo.com

CC;

Janet S. Clarke, BOS Vice Chairman, Blue Ridge District
Ralph M. Buona, BOS Ashburn District
Geary M. Higgins, BOS Catoctin District
R. Kevin Ruedisueli, Planning Commission Chairman

David Ward

From: Patti Psaris [pattipsaris@rstarmail.com]
Sent: Tuesday, May 15, 2012 9:33 AM
To: dward@earthwardconsulting.com
Subject: FW: Loudoun County Scenic River Advisory Committees - Mapping Requests Letter May 14, 2012
Attachments: Scenic River Advisory Committees - mapping request - May 14 2012.pdf

David – Please put this letter in our “cloud” files (replace the draft that is on there!)

Thank you!
Patti

From: Patti Psaris [<mailto:pattipsaris@rstarmail.com>]
Sent: Tuesday, May 15, 2012 9:31 AM
To: 'scott.york@loudoun.gov'
Cc: 'janet.clarke@loudoun.gov'; 'ralph.buona@loudoun.gov'; 'kevin.ruedisueli@loudoun.gov'; 'geary.higgins@loudoun.gov'
Subject: Loudoun County Scenic River Advisory Committees - Mapping Requests Letter May 14, 2012

Dear Supervisor York,

Attached please find a letter from the Catoctin Creek Scenic River Advisory Committee and the Goose Creek Scenic River Advisory Committee. This letter is a request for action and includes several suggested actions to improve the County's mapping activities for our scenic rivers. We believe these actions will benefit the County and the public, as well as our committees.

With appreciation for your support of Loudoun County's Scenic Rivers – Thank you!
Patti

Patti J. Psaris, Chair
Catoctin Creek Scenic River Advisory Committee
Creating a community of stewardship for the Catoctin Creek
CatoctinScenicRiver@gmail.com
Watershed address:
South Fork Catoctin, Catoctin Creek, Potomac, Chesapeake Bay

Section 5-1000

Scenic Creek Valley Buffer.

5-1001

Purpose and Intent. The Scenic Creek Valley Buffer is established to govern the construction of buildings, structures, parking, and other impervious surfaces in areas adjacent to scenic rivers and all waterways draining greater than 640 acres, by providing for a setback area from the channel scar line in which construction of improvements would not occur except as set forth below. The intent is to (1) promote water quality and the preservation of significant environmental resource areas, wildlife habitat and corridors, and native vegetation areas; (2) protect and enhance water and groundwater recharge processes by protection of the natural capacity of vegetative areas along rivers and creeks to filter and purify storm water runoff; (3) protect aquatic environments from the warming effects of solar radiation by preserving riparian tree canopy cover; (4) promote tourism and high quality corporate investment by maintaining to the extent reasonably possible, existing high water quality; (5) to maintain the scenic beauty of the streams of Loudoun County; and (6) implement the Comprehensive Plan.

5-1002

Scenic Creek Valley Buffer Established. The following setbacks are established along all waterways draining greater than 640 acres in areas where the major 100 year floodplain is less than the setbacks provided below.

- (A) 250 feet measured along the slope of the ground from the channel scar line on the Potomac River.
- (B) 200 feet on each side of the creek measured along the slope of the ground from the channel scar line of the Scenic River designated portions of Goose Creek and Catoctin Creek.
- (C) 150 feet on each side of the creek measured along the slope of the ground from the channel scar line of each creek or stream where the watershed is greater than 640 acres.
- (D) The above setbacks may be reduced as follows:
 - (1) A reduction of 100 feet shall be allowed for the retention of an existing forested area or the creation of a forested area, as approved by the Area Forester as part of a management plan between the ultimate setback line and the channel scar line; or
 - (2) A reduction of 100 feet shall be allowed for the use of and retention of stormwater management/BMP practices in accordance with the FSM at time of development within any developed area on the lot or site.
- (E) The above setback does not apply to agricultural, horticultural, or forestal uses where a farm plan approved by the Loudoun County Soil and Water Conservation District or other County approved agency is kept continuously in place.

- 5-1003** **Effect of Buffer.** The construction of buildings, structures, parking lots, or other impermeable surfaces within the Scenic Creek Valley Buffer is prohibited, except as stated herein. Existing buildings and structures within the Scenic Creek Valley Buffer are not considered nonconforming, i.e., they can be added to and, if destroyed by fire or casualty, they can be rebuilt to the same or an equivalent footprint. This buffer or setback area does not regulate uses within the setback area, although the County encourages the growth, through plantings or natural succession, of vegetative and forestal cover within the Scenic Creek Valley Buffer area. Utilities may be located within the buffer.
- 5-1004** **Existing Lot Criteria.** On any existing lot of record as of June 16, 1993, one (1) single family residence and its attendant unpaved driveway, unpaved parking area, and/or detached garage and incidental structures cited in Section 4-1500 shall be permitted within the setback area.
- 5-1005** **Development Criteria.** The Scenic Creek Valley Buffer is not intended to, and shall not, limit development density (gross floor area or units per acre) otherwise allowed on land within the Scenic Creek Valley Buffer area. The Scenic Creek Valley Buffer shall be administered like any other setback provided for in this Ordinance in allowing otherwise developable land within the setback area to be counted for density computation purposes and applied toward the construction of improvements outside the setback area. Road crossings and driveways, shall be permitted subject to applicable federal and state regulations, to this Ordinance, and to such performance standards as may be contained in the Facilities Standards Manual.



PIEDMONT ENVIRONMENTAL COUNCIL

*Safeguarding the landscape, communities and heritage of Virginia's Piedmont by involving citizens in
Related public policy and land conservation*

Raymond Ezell, RPA
Virginia Department of Transportation
87 Deacon Road
Fredericksburg, VA 22405
Raymond.Ezell@vdot.virginia.gov

Re: John G. Lewis Bridge, Featherbed Ln Bridge over Catoctin Creek, DHR file 2014-0426

Dear Mr. Ezell:

On behalf of The Piedmont Environmental Council (PEC), I am providing the following comments as a part of the 106 consultation process for the John G. Lewis Bridge in Loudoun County. PEC was formed in 1972 for the purpose of preserving and protecting the natural resources, rural economy, history, and beauty of the Virginia Piedmont. We have played an active and integral role in preserving the Commonwealth's historic resources and landscape in our 9-county region for over 40 years.

PEC's primary goal in this process is to reach a determination on the potential for restoration of the bridge and evaluate the various options designed to retain the structure's listing on the National Register of Historic Places (NRHP). We appreciate VDOT's efforts to provide options to address the current condition of the John G Lewis Bridge. However, we believe there has been a rush to reach consensus on a solution and that the agency has failed to provide adequate information to make an informed decision. VDOT has provided little evidence and shown no external peer review as to how they reached the determination that rehabilitation for the continued use as a vehicular bridge, which would assuredly retain the listing of the resource on the NRHP, is no longer possible.

In the agency's haste to progress the project, several steps in the procedure appear to have been

overlooked or postponed. This has actually caused delay as these steps may have provided information that would assist the community in reaching a conclusion on the options - those presented and those yet to be fully evaluated. Also of concern, none of the options provided by the agency include restoration and

rehabilitation for vehicular traffic. The one option that retains the bridge without any major changes, eliminates its historic use as a vehicular bridge and ultimately changes the character of the area to the point where the listing may be threatened. While challenges have been identified, including the possibility for damage resulting from the metalizing treatment instituted in 2003, the agency has been unable to prove existing issues were the result of metalizing or if any of the cracks would prevent the repair and continued use of the bridge. VDOT has not even been able to tell us the material used in the bridge (iron or steel) construction, which would also give some insight on the potential for rehabilitation and restoration.

It has been a point of frustration that the agency has failed to involve the VA Department of Historic Resources (VDHR) nor provide information on what effect the options presented would have on the status of the resource as it pertains to the listing on the National Register of Historic Places. As this is a 106 proceeding, we would have thought we would have received some input from the SHPO and VDHR by this point. It was only by contacting VDHR that we were made aware that VDOT had not involved the SHPO in any meaningful way thus far. We were please to see the correspondence from VDHR expressing their desire to be a part of the conversation.

36 CFR 800 provides that the SHPO will cooperate “...to ensure that historic properties are taking into consideration at all levels of planning and development.” Consultation shall be initiated in a manner appropriate “...to the nature of the undertaking and its effects on historic properties.” Given the solution employed may affect the listing on the Register, we thought the SHPO should have already been engaged by this point. The code states: “An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association.” Given that all options presented by the agency at this time would likely constitute an adverse effect, we eagerly await the invitation of the SHPO and VDHR to review all pertinent documents related to the project and their participation at any future meeting of the consulting parties.

We appreciate this opportunity to provide comments on this project and look forward to continued

discussion as it pertains to the section 106 proceeding.

Sincerely,

Daniel Holmes
Director of State Policy
Piedmont Environmental Council
dholmes@pecva.org
(540) 347-2334



John G. Lewis Memorial Bridge Route 673 (Featherbed Lane) Bridge over Catoctin Creek in Loudoun County

National Register Listing “Catoctin Creek Bridge”

Presentation March 26, 2015, July 23, 2015
& September 16, 2015



What we know.

- The Bridge is on the National Register of Historic Places.
- Section 106 of the National Historic Preservation Act of 1966 requires that Federal agencies allow the Advisory Council on Historic Preservation an opportunity to comment on all projects affecting historic properties either listed in or determined eligible for listing in the National Register.
- Qualification for Federal grants for historic preservation, when funds are available.



“John G. Lewis Memorial Bridge”

Historical Significance

(extrapolated from the National Register Listing)

- **January 25, 1974 – Entry Date**
- “Modern guardrails are located along the sides but the wooden plank roadbed is intact.”
- “The ends are set on fieldstone abutments.”
- “On the Eastern End is a damaged circular plaque reading:
“The Variety Iron Works Cleveland O. Bridge Builder.”
- The condition blocks are checked as **Good** and **Unaltered**



“John G. Lewis Memorial Bridge”

Statement of Significance

“The Catoctin Creek Bridge is an excellent example of the metal truss bridges once prevalent on the secondary roads throughout the state. Like the covered bridges they superseded, the metal truss bridges are a fast disappearing piece of Americana. This particular example is virtually the only bridge of its size and type left in northern Virginia, and it is given added distinction by its unusually picturesque setting in the beautiful farming region near the Quaker community of Waterford. The shaded unpaved county road served by the bridge, and wooden plank roadbed on the bridge itself add nostalgia to the scene.

The bridge was produced by the Variety Iron Works of Cleveland, Ohio, around 1900. It originally was located on Route 7 across Goose Creek east of Leesburg. It was dismantled and moved to its present location around 1932.”



“John G. Lewis Memorial Bridge”

Alterations / Changes over the Years

- Per the Baker inspection report, and as noted in the National Register, The bridge was originally built in 1889 on the Leesburg & Alexandria Turnpike (Route 7) over Goose Creek. In 1932 it was dismantled and moved to its present location.
- In 1967 the stringers were replaced.
- Truss joints were retrofitted and several truss bracings were replaced.
- Guardrail was added continuously across the bridge.
- The bridge was metalized thus changing its appearance from original. (2003)
- After damage by a fallen tree 2 eye-bar members were replaced. (2003)
- Numerous other retrofit details were applied during rehabilitations. (2003)
- Stone masonry abutments have been capped and pointed. (2003)
- Roller type bearings have been replaced. (2003)



“John G. Lewis Memorial Bridge”

Stringers on Abutment



Before Metalizing



After Metalizing



“John G. Lewis Memorial Bridge”



Tree Impact on Metalized Truss
(Reconstruction in 2003)



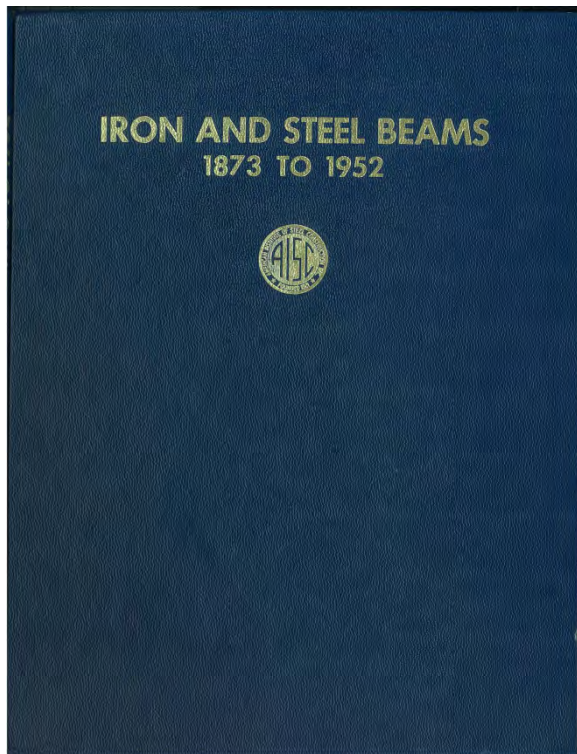
Current Structural Issues

- Chemical composition of the material (steel or wrought iron?) is unknown, thus the susceptibility to brittle fracture is unknown. What is known is that it was manufactured in 1889.



“John G. Lewis Memorial Bridge”

Wrought Iron or Steel? Built in 1889?



EARLY UNIT STRESSES USED IN TABLES OF ALLOWABLE LOADS AS PUBLISHED IN CATALOGS OF THE FOLLOWING MILLS

FOR WROUGHT IRON

Year	Rolling Mill	Unit Stress
1873	Carnegie Kloman & Co. ("Factor of Safety 3")	14000 psi
1874	New Jersey Steel & Iron Co.	12000 psi
1881-1884	Carnegie Brothers & Co., Ltd.	{12000 psi 10000 psi
1884	The Passaic Rolling Mill Co.	{12000 psi 10000 psi
1885	The Phoenix Iron Company	12000 psi
1885-1887	Pottsville Iron & Steel Co.	12000 psi
1889	Carnegie Phipps & Co., Ltd.	{12000 psi 10000 psi

FOR STEEL

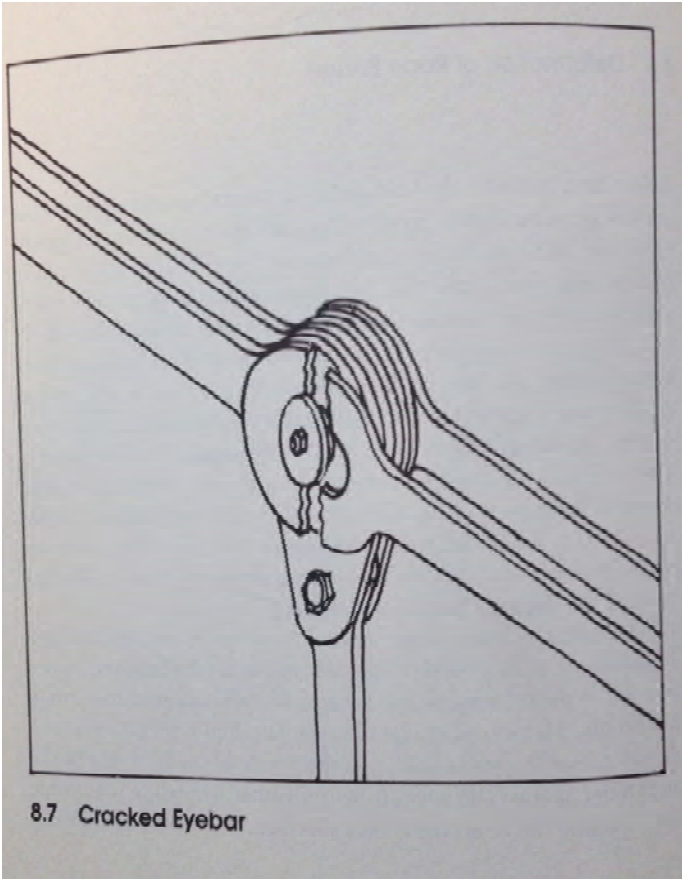
1887	Pottsville Iron & Steel Co.	15600 psi
1889-1893	Carnegie Phipps & Co., Ltd. (Bldgs.) (Bridges)	{16000 psi 12500 psi
1893-1908	Jones & Laughlins Ltd. } Jones & Laughlin Steel Co. }	{16000 psi 12500 psi
1896	Carnegie Steel Co., Ltd. (Bldgs.) (Bridges)	{16000 psi 12500 psi
1897-1903	The Passaic Rolling Mills Co.	{16000 psi 12000 psi
1898-1919	Cambria Steel Co.	{16000 psi 12500 psi
1900-1903	Carnegie Steel Company (Bldgs.) (Bridges)	{16000 psi 12500 psi
1907-1911	Bethlehem Steel Co. (Bldgs.) (Moving loads)	{16000 psi 12500 psi
1915	Lackawanna Steel Co.	{16000 psi 12500 psi

Current Structural Issues

- The bridge is a fracture critical structure with 2 – eye bar chains for the lower chord. Should one eye-bar fail, doubling the load on the 2nd eye bar, it too would be expected to fail.
- Eye-bars of this vintage typically would have high carbon content and fewer alloys thus making them more brittle and susceptible to brittle fracture.



“John G. Lewis Memorial Bridge”



Current Structural Issues (cont.)

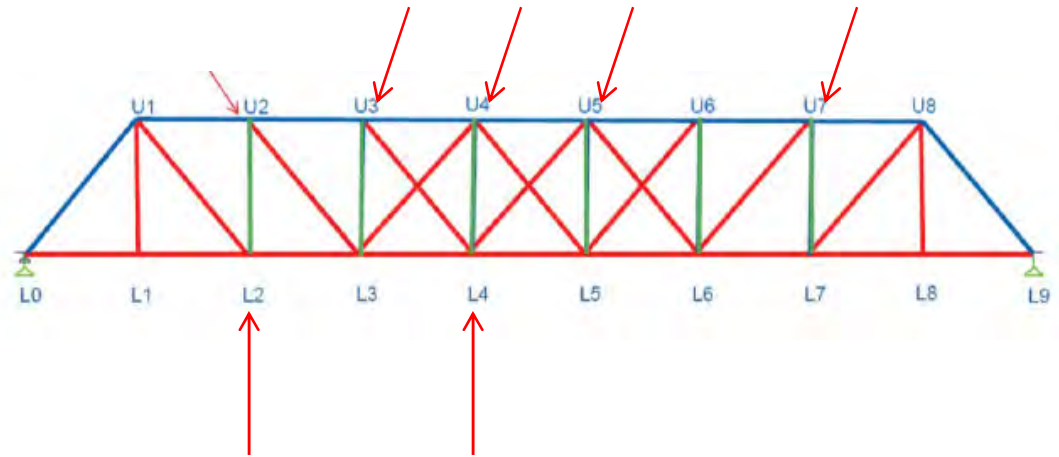
- Metalizing may have altered the surface metal properties.
- Inspection of the pin connections, is difficult for inspectors and requires additional inspections.
- The structure has noticeable loss of section (pitting)
- The structure has a mixture of rivets, machine bolts (unacceptable structurally) and H.S. Bolts.
Some bolts have improper thread lengths.
- The abutments are/were stone masonry.



“John G. Lewis Memorial Bridge”

Cracking has been prevalent on the structure and can now be observed at:

1. Joint U2N truss member,
2. Joint U3N several locations,
3. Joint U7N,
4. Near U4N,
5. Near U2S,
6. Upper Chord outer web near U5 downstream
7. Floorbeam support plate at L2 Downstream
8. Floorbeam support plate L4 Downstream,
9. In the bracing plate at U4 Downstream,
10. U2 angle bracket upstream,
11. Lateral bracing connection at L4 upstream.



“John G. Lewis Memorial Bridge”



“John G. Lewis Memorial Bridge”



Location of Crack

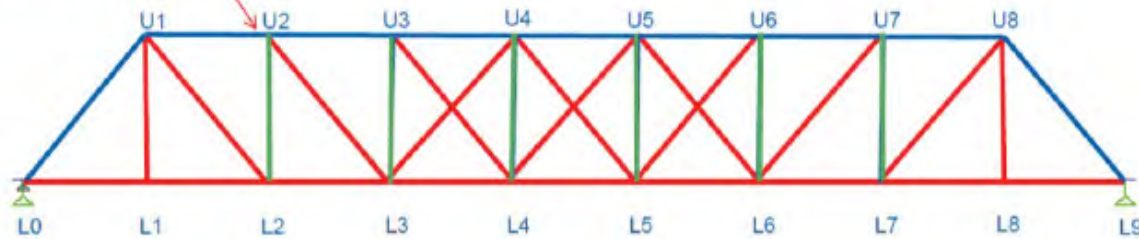


Exhibit 11. Cracking in Upper Chord



“John G. Lewis Memorial Bridge”



“John G. Lewis Memorial Bridge”

Joint U7 N



Near U4 N



Near U2 S



PHOTO 27 - CRACK IN LATERAL BRACING CONNECTION WASHER AT L4, UPSTREAM



“John G. Lewis Memorial Bridge”



PHOTO 12 - CRACK IN UPPER CHORD OUTER WEB NEAR U5, DOWNSTREAM



“John G. Lewis Memorial Bridge”

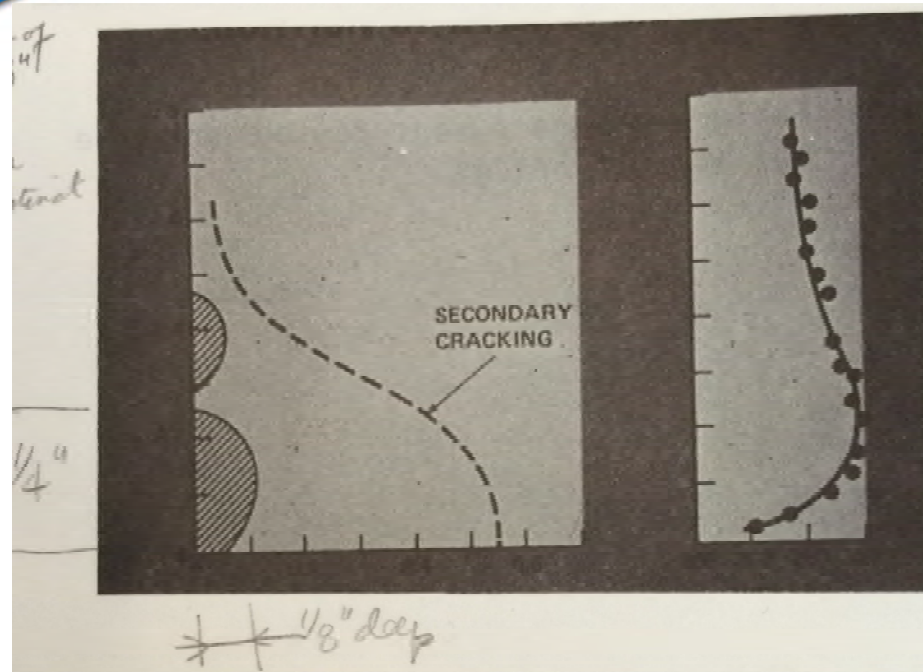


“John G. Lewis Memorial Bridge”

Fatigue, Fracture and Crack Propagation



“John G. Lewis Memorial Bridge”



21
"John G. Lewis Memorial Bridge"

Posting Limits

Year	Posted Limit
2003 Rehab	15 Tons
2008	8 Tons
2014	3 Tons



“John G. Lewis Memorial Bridge”

POSTING LIMITS...

What posting limits? I never saw that sign!



“John G. Lewis Memorial Bridge”

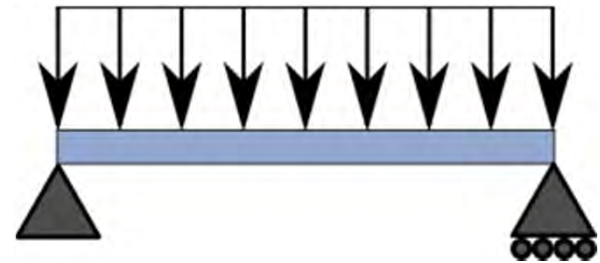
Geometric Issues

- The existing roadway width is only 11' – 2"
Thus making the structure...
Functionally Obsolete.

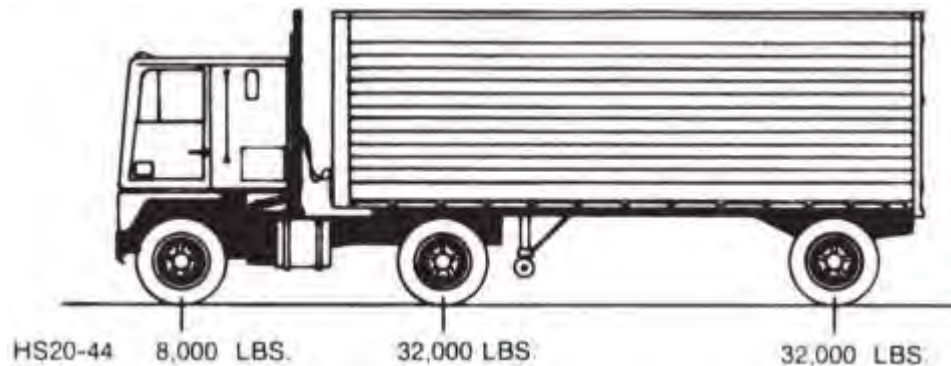


“John G. Lewis Memorial Bridge”

Design Live Load ???



100 Lbs./SF



25
“John G. Lewis Memorial Bridge”



What to Do?

Guiding Principles

Mission Statement

Our mission is to plan, deliver, operate and maintain a transportation system that is **safe**, enables easy movement of people and goods, enhances the economy and improves our quality of life.



“John G. Lewis Memorial Bridge”

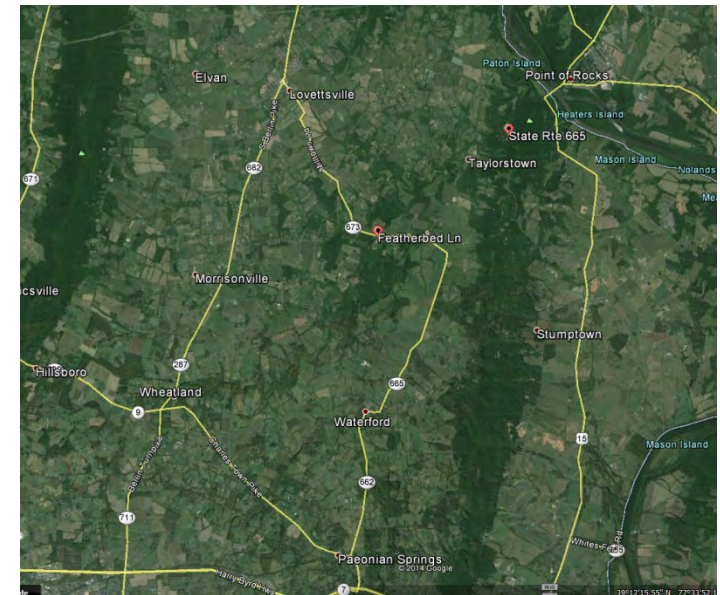
What to Do?

Guiding Principles

1. Safe – both structurally and functionally
 - Provides for the movement of People and Goods
 - Provides a 75 year service life
 - Recognizes the Historical Significance of the Existing Bridge
 - Is serviceable
 - Makes the best use of limited resources
 - Enhances the community
 - Others



“John G. Lewis Memorial Bridge”



Detour Route

Is 11.5 miles long and, per Bing, is approximately a 25 minute drive.



“John G. Lewis Memorial Bridge”

Options for Discussion

- Do Nothing. This could mean closing the bridge.
- Repair the existing bridge, modified with a redundant load path. (i.e. supplemental support structure)
- Replace the bridge with a new structure in the same location.
- Build a parallel structure.
- Preservation of the bridge in some form/location.
- Others?



“John G. Lewis Memorial Bridge”

Responses to Nathan Holt's Email dated July 23, 2015



“John G. Lewis Memorial Bridge”

1. You indicated that you had a different consultant inspect the bridge. Why was a different consultant used? Did this new consultant have experience in the in-kind restoration of historic metal truss bridges including splicing replicated member sections, pad welding for section loss, pneumatic pack rust removal, hot metal riveting, etc?

Answer: The new consultant was not inspecting the bridge for the purpose of rehabilitation /restoration. The inspection was an annual safety inspection. Because this bridge is deemed “Fracture Critical” we are required to perform a hands-on, arms length inspection annually versus every two years as we do with all other structures.



31
“John G. Lewis Memorial Bridge”

2. At the outset of Section 106 Consultation, I requested VDOT to supply me with relevant bridge inspection reports so I could better understand the bridge's problems. The reports I was supplied are dated February 18, 2014, and July 11, 2014. If there has been a more recent inspection report as you have stated, I would like to request that the report be supplied to me as well so I may effectively perform my duties as a Consulting Party. As I did for the first two reports, I will be happy to sign a confidentiality agreement if needed.

Answer: We will provide any and all data that we have available. Nothing is being withheld. The new information is from an inspection done on February 18, 2015 we received the validated report in June.



32
“John G. Lewis Memorial Bridge”

3. You stated very clearly (with underlining for emphasis) in your statement that the **very next day** after rehabilitation this bridge would have more cracks. This is an incredible, extraordinary claim, and I would appreciate elaboration on why this is the case. Equally incredible and extraordinary is your statement that the non-destructive disassembly of the bridge would cause even more cracking. What is so unique about this bridge to cause such extraordinary rate of cracking? And what is unique about this bridge that the routine process of non-destructive disassembly would induce further cracking?

Answer: “It doesn’t matter if the material is steel or wrought iron. Whatever it is...its cracking. We could fix all of those cracks and call it rehabilitated and the very next day have more cracks.”

The bridge is unique in that it has been relocated, it has been metalized, it subject to more freeze thaw cycles and thus brittle fracture. Non-destructive disassembly is not so non-destructive!



33
“John G. Lewis Memorial Bridge”

4. Your comments almost make it sound like you don't know why the bridge has cracks and continues to show cracks. Surely that isn't the case? Please detail the type and cause of the cracking. Is it the result of something extremely rare such as "unusually temperature sensitive brittle steel" as is the case with the Ironton Russell Bridge over the Ohio River? Or are the cracks something more common, like fatigue cracks caused by overloading of the structure, and/or excessive loading cycles of the metal? Might some of the cracks be located near rivet holes, indicating cracking initiated from punched holes for rivets?

Answer: We don't know why it has cracked. The cracks are in tension members, compression members, primary and secondary members. We have speculation that the cause could be due to the metalization. From the June 2013 consultant report prepared by Michael Baker Corporation:

"POTENTIAL CAUSES OF CRACKING

The following factors were investigated as potential causes of cracking:

- 1) Truck Overloads
- 2) Wind Load & Torsion
- 3) 2003 Tree Impact during Reconstruction
- 4) 2003 Joint Retrofit
- 5) 2003 Metalizing for Corrosion Control"



34
"John G. Lewis Memorial Bridge"

“Process of Metalizing : Metallizing is basically a method of galvanizing. It refers to the thermal spraying Of zinc (or aluminum alloys) as a coating directly onto steel surfaces. The coatings are created by using a heat source (either flame or electric arc) to melt the metal which is supplied as a wire. An airstream sprays the molten metal onto the steel surface. Once the molten metal strikes the steel it solidifies quickly to become a coating. Metalizing is applied on a prepared/cleaned surface. Surface preparation is typically done by abrasive blasting. Chemical etching sometimes has been used for surface preparation.”



35
“John G. Lewis Memorial Bridge”

“Steel Embrittlement: There have been cases in the past that galvanizing was believed the reason for steel embrittlement and cracking. Cold working is the strongest factor contributing to the Embrittlement of galvanized steel. In cold worked steel, galvanizing significantly accelerates the strain aging and embrittlement. Chemical etching for surface preparation is another factor that may cause embrittlement, depending on the chemistry of the steel. Hydrogen embrittlement of steel in the process of galvanizing is, yet, another factor which is mainly associated with high strength steel. ASTM Recommended Practice A1 43, “Safeguarding Against Embrittlement of Hot Dip Galvanized Structural Steel Products and Procedure for Detecting Embrittlement” provides guidance against embrittlement.”



36
“John G. Lewis Memorial Bridge”

“Role of Metalizing in Cracking:

Information on the properties of the steel used in 1889 in the Construction of Featherbed Lane Bridge is not available, to conclude if the metalizing (galvanizing) has Caused embrittlement and cracking at U2 joint. However, the supplementary inspection of the bridge performed on April 9, 2013 revealed the presence of cracking in many locations in the truss, indiscriminately, indicating that the cracking phenomenon is related to a factor that applies to the overall truss, rather than a particular location. [See Exhibit17.] This in turn suggests that the metalizing performed on the overall truss in 2003 have most likely had a role in the occurrence of cracking.”



37
“John G. Lewis Memorial Bridge”

Aden Truss: We were planning to metalize the Aden Truss. We are not.

We reached out to experts:

- John Barsom, Ph.D. Former Director of Research at United States Steel
- Reidar Bjorhovde, Ph.D. World renowned structural steel consultant. He authored all of the compression formulas used in codes today.
- Volkert has sought opinions from a variety of sources in industry associated with the rehabilitation of iron bridges. These sources include the FHWA, US Bridge and the Coatings Industry. All of the sources are in agreement that galvanizing wrought iron is a difficult and unknown process.

The chemical make-up and manufacture of wrought iron is different from that of steel. Wrought iron will typically contain less than 0.1% carbon while steels will range from 0.3 to 0.6% carbon. The manufacturing processes of that day were inconsistent which made the control of the levels of carbon and other impurities difficult. The result is that the chemical makeup of the finished product can be inconsistent. That could mean that the chemical makeup of individual members of a truss could be different and therefore produce varying visual and bonding effects when applying coatings.

According to industry, the current galvanization process is set up for modern day structural steels. Structural members are blast cleaned, put through an acid bath, rinsed a number of times, pre-flux agent applied and dipped in molten zinc. Because of the inconsistencies in the chemical makeup and manufacturing process of wrought iron, galvanizing is unpredictable. The only way to know is to test a sample piece/ pieces.



38
“John G. Lewis Memorial Bridge”

5. Regarding cracks, if the cracks are on minor elements such as batten plates, or bent plate used to provide an attachment for lateral bracing rods, etc, it should be noted that these elements are not major structural members and can be replaced in-kind by a fabricator easily at low cost. If there are cracks at a common trouble spot which is the field bolted splice(s) of the upper chord, this problem can be corrected by fabricating a couple replica sections of upper chord, cutting the cracked portion out, and welding the replicas in as replacement. This process was completed in Fayette County, Texas for a bridge called the Piano Bridge. Please see attached photos of the Piano Bridge showing the problem and the fix.

Answer: The cracks are on many different members. Replacement-in-kind is not non-destructive. It can be very destructive! It is not that easy. It is costly.



39
“John G. Lewis Memorial Bridge”



40
“John G. Lewis Memorial Bridge”

6. Considering the aforementioned about cracking, can you please indicate specific areas of cracking on the bridge that are not simple, easily-replaced elements like batten plates, plates or washers for attachment of lateral bracing, u-bolt connection plates.... and also are not cracks associated with the field bolted upper chord splice?

Answer: We have provided all previous reports. We will provide the new report. The issue isn't so much where the cracks are now...it is where will they be next? We are tasked with searching for miniscule cracks, before they become large cracks.



41
“John G. Lewis Memorial Bridge”

7. Please elaborate on the metalizing of the bridge. I am familiar with the traditional three coat paint system that DOT's use today, and I am familiar with lead paint traditionally used on bridges in decades past, and I am also aware that some states including I believe Virginia have even hot dip galvanized trusses. However, I admit that I know very little about "metalizing," and would appreciate elaboration on what this process is, and in particular how this process might lead to structural cracking in a truss. You also indicated that you cannot undo the metalizing. Are you saying it is impossible to blast this bridge down to bare metal?

Answer: Previously discussed.



42
"John G. Lewis Memorial Bridge"

8. You made the statement that the bridge is “not safe to rehabilitate” and immediately followed that statement with the statement that the bridge “also has dual eye-bars and uninspectable pin details.” My experience is that nearly every major rehabilitation of a pin-connected truss bridge that I have been involved with included the replacement of existing pins. I agree... there is no good way to inspect pins for problems, even ultrasonic testing is problematic. Replacing pins is common, and not considered a major alteration, especially if the new pins replicate the dimensions and threading of the original pins, and if stainless steel pins are used, these are painted so they don’t stick out visually. I generally find that if mild steel is used, pins run from \$500-\$600 per pin. The cost would, of course, be higher for stainless steel. But my point is that pin replacement is not unusual, costly, or detrimental to maintaining the historic integrity of the bridge.

Answer: The pins are not the issue. The problem area is the eye bar surface around the pin. Remember the “Point Pleasant Bridge aka Silver Bridge” in West Virginia.



43
“John G. Lewis Memorial Bridge”

9. Next, in regards to your statement about dual eyebars, are you suggesting that any truss bridge with dual eye-bars is automatically unsafe, because that would basically be calling into question the safety of numerous bridges rehabilitated by numerous DOTs across the country following stamped engineering plans. That's a pretty big charge. Nearly every pin-connected highway truss in existence consists of paired eyebars for tension members, and numerous rehabilitation projects across the country maintain the paired eyobar configuration for these members. That said, the addition of post-tensioning cables in between existing eyobar pairs to provide redundancy would be a minimal alteration from a visual standpoint, and I would be happy to support a proposal of this type if that made VDOT more comfortable... this certainly would be more preferable than welding enormous load-bearing girders to the truss!

Answer: Yes it is a big charge and I standby my statement.

Adding cable redundancy is an option and we most likely would do so if we extend the life of the bridge for pedestrian use.



44
“John G. Lewis Memorial Bridge”



45
“John G. Lewis Memorial Bridge”

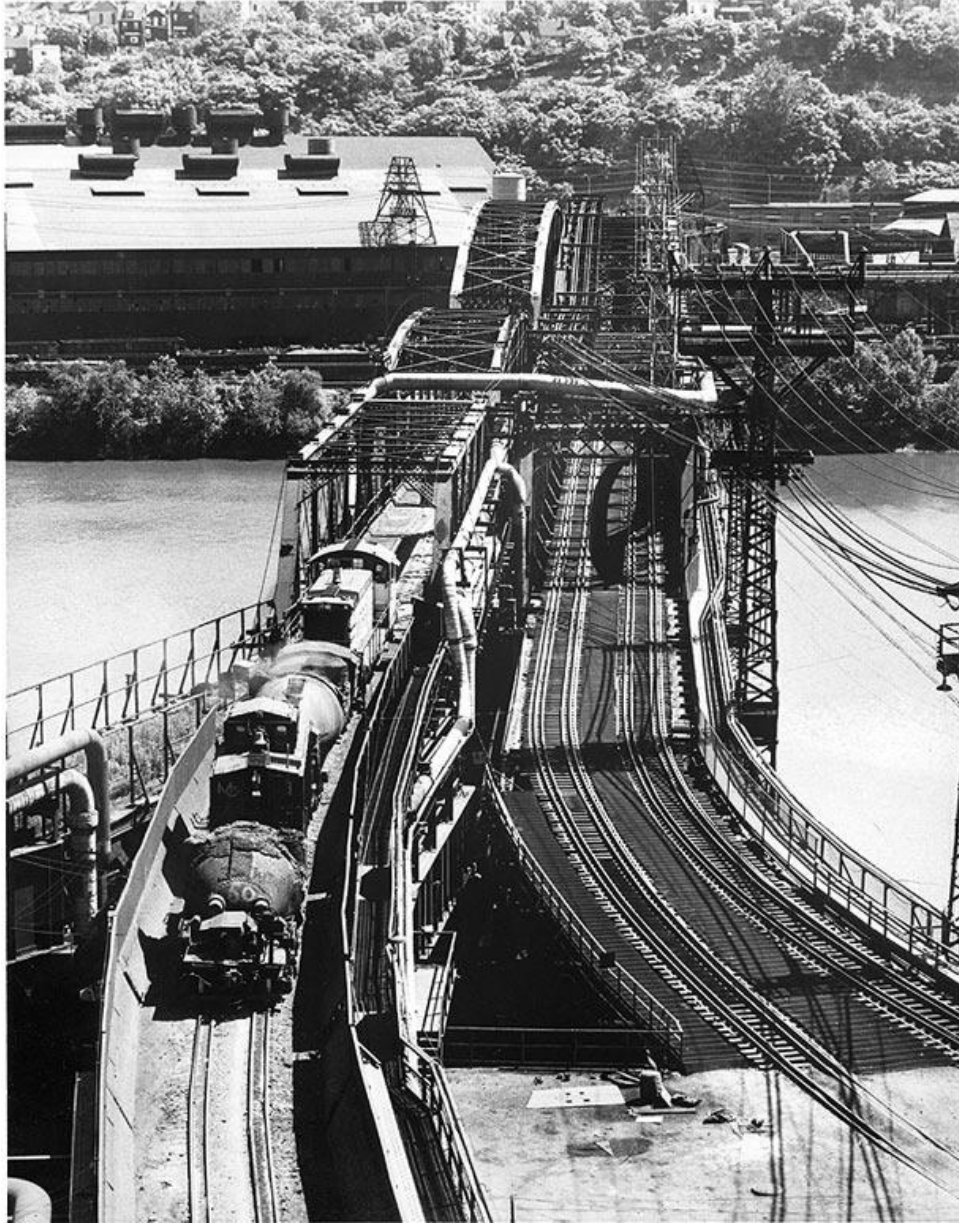
10. You stated that you cannot stop overloads on the bridge. The installation of clearance-limiting “headache bars” over the approaching roadway is an effective way to keep trucks off the bridge. Additionally, if the intent is to provide for the typical usage of this bridge by light vehicular traffic (residential traffic / passenger cars), I would expect that these loads would not put the bridge into the same type of loading cycles that trucks do, meaning fatigue cracks would not initiate or propagate at the same rate as they are with concrete trucks driving over the bridge. A combination of headache bars and posted weight limits may be an effective way to prevent fatigue-inducing loads from crossing the bridge.

Answer: Headache bars can also give headaches to horseback riders, and farmers on tractors.

? Light residential use...Does this include Refuse Collections, Concrete Trucks, Moving Vans, Truck Drivers who get lost? How policed?



46
“John G. Lewis Memorial Bridge”



The Hot Metal Bridge and the Monongahela Connecting Railroad Bridge. Built circa 1900.



47
“John G. Lewis Memorial Bridge”





Evaluation of Strengthening/Replacement Alternatives John G. Lewis Memorial Bridge on Route 673 (Featherbed Lane) over Catoctin Creek

September 16, 2015

Vicente Valeza, P.E.

NOVA District Senior Structural Engineer

(703) 259-3256

Vicente.Valeza@vdot.virginia.gov

Link to the project webpage:

http://www.virginiadot.org/projects/northernvirginia/route_673_over_catoctin_creek.asp

Alternative 1

Features

Provide new 2-span continuous thru-girders along the fascia of the existing bridge to support the existing truss bridge

- Existing truss to remain: 158' span steel pin connected Pratt through truss with new glu-lam timber deck and floor beams
- Restores 15 ton vehicular capacity
- Maintain one-lane bridge (11'-2" clear width) with 2-way traffic
- Functionally Obsolete bridge roadway width (12' min.) per Chapter 32 of the VDOT BDM Vol. V, Part 2
- New pier and abutment widening
- Estimated Cost \$1.6M to \$2.6M

Alternative 1

Add Steel Thru Girders to Fascia of Existing Truss Bridge

John G. Lewis Memorial Bridge over Catoctin Creek

ALTERNATIVE 1 - Add Steel Thru Girders to Fascia of Existing Truss Bridge



Alternative 2

Features

Replace bridge with new two-span continuous steel beam bridge (80'-80') spans with glu-lam timber deck

- Attach existing truss members to the new bridge to maintain the appearance of the existing truss bridge
- Provide one-lane bridge (14' clear width) with 2-way traffic
- Eliminates Functionally Obsolete bridge roadway width (12' min.) per Chapter 32 of the VDOT BDM Vol. V, Part 2
- Designed for AASHTO LRFD HL-93 loading plus weight of truss members
- New Pier and Abutments
- Estimated Cost \$3.0M to \$4.0M

Alternative 2

New Two-Span Continuous Steel Beam Bridge with Existing Truss Members Attached

John G. Lewis Memorial Bridge over Catoctin Creek

ALTERNATIVE 2 - Two-Span Continuous Steel Beam and Timber Deck Bridge with Existing Truss Members Attached. Widened Bridge by 3 ft. for Guardrail Deflection Clearance



Alternative 2A

Features

Replace bridge with new two-span continuous steel beam bridge (80'-80') spans with glu-lam timber deck

- Attach existing truss members to the new bridge to maintain the appearance of the existing truss bridge
- Provide one-lane bridge (10'-2" clear width) with 2-way traffic
- Does not eliminate Functionally Obsolete bridge roadway width (12' min.) per Chapter 32 of the VDOT BDM Vol. V, Part 2
- New steel beams designed for minimum 15 ton vehicular capacity
- New Pier and Abutments
- **Estimated Cost \$3.0M to \$4.0M**

Alternative 2A

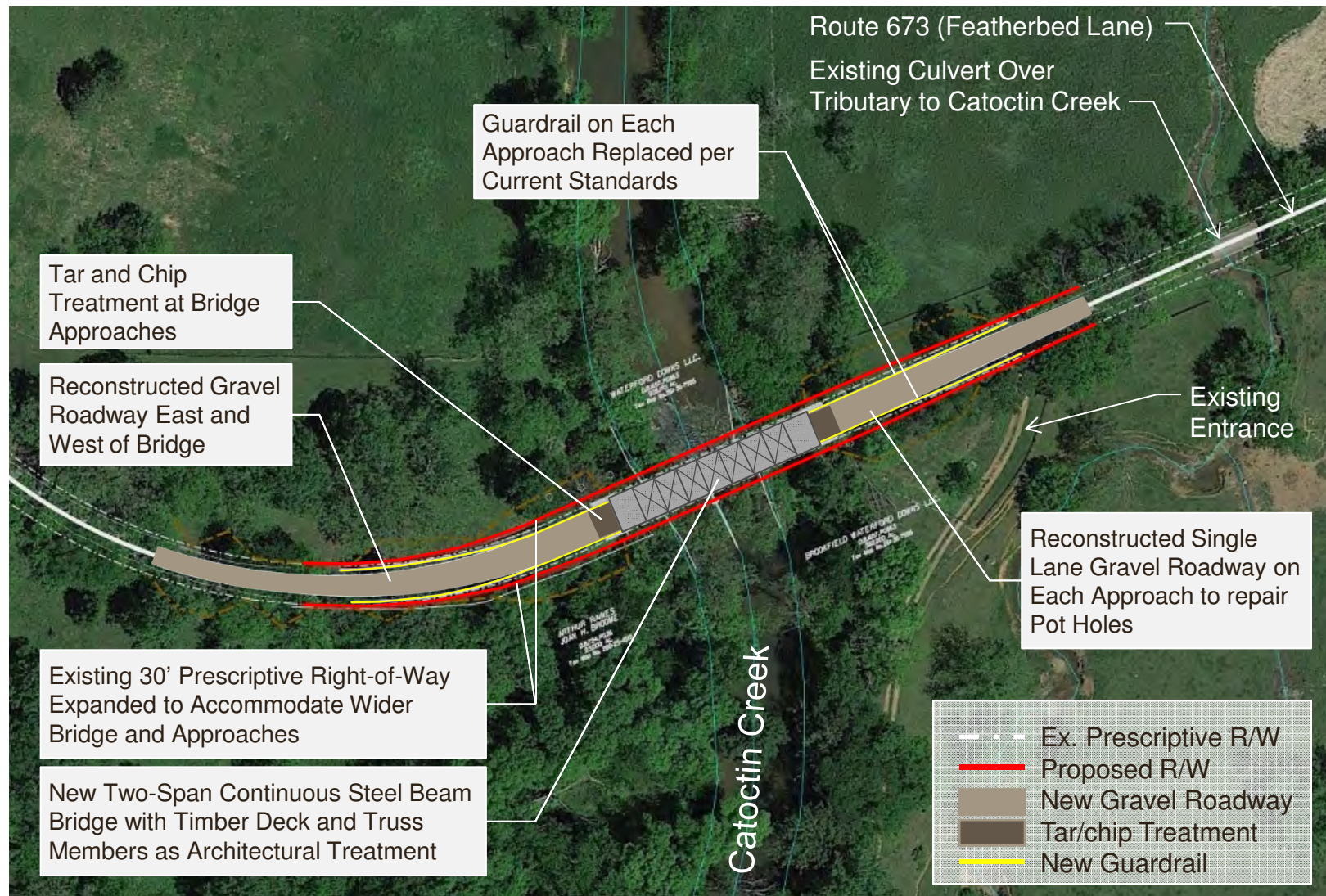
New Two-Span Continuous Steel Beam Bridge with Existing Truss Members Attached

John G. Lewis Memorial Bridge over Catoctin Creek

ALTERNATIVE 2A - Two Span Continuous Steel Beam and Timber Deck Bridge with Existing Truss Members Attached. Maintain Existing Bridge Width



Alternative 2 – Roadway Plan



Alternative 3

Features

Provide internal arch along existing truss members to restore 15 ton vehicular capacity and add structural redundancy

- Existing truss to remain: 158' span steel pin connected Pratt thru truss with new glu-lam timber deck
- Maintains one-lane bridge (11'-2" clear width) with 2-way traffic
- Does not eliminate Functionally Obsolete bridge roadway width (12' min.) per Chapter 32 of the VDOT BDM Vol. V, Part 2
- Requires additional steel arches, hangers, floor beams and bottom tension cables
- Abutment Widening
- Estimated Cost \$1.5M to \$2.5M

Alternative 3

Add Steel Internal Arch to Existing Truss Bridge

John G. Lewis Memorial Bridge over Catoctin Creek

ALTERNATIVE 3 - Add Steel Internal Arch to Existing Truss Bridge



Alternative 4

Features

Replace existing Pratt thru truss bridge with a new similar, wider Pratt through truss bridge

- Will carry two lanes, one lane each direction
- Meets minimum GS-4 criteria with two 10' lanes and 1' offsets to the railing on each side
- Designed for AASHTO LRFD HL-93 loading
- New Abutments
- Estimated Cost \$3.0M to \$4.0M

Alternative 4 – View 1

New Steel Thru Truss Bridge



Alternative 4 – View 2

New Steel Thru Truss Bridge



Alternative 4 – View 3

New Steel Thru Truss Bridge



Similar Historical Truss Bridge Replacement Milton Street Bridge Replacement, Warren County, NY

Technical Description:

- Width: 29' 7"
- Span: 155'
- Style: Freedom Series Thru Truss
- Finish: Weathering Steel
- Decking: Concrete

Installation Date: Fall 2011

- Provided by U.S. Bridge



Highlights:

The original 100 year old steel truss bridge was closed to traffic in 2008 for safety reasons.

The County decided to replace the narrow one-lane Bridge with a wider and safer two-lane bridge. The County wanted to keep the same “feel” of the old historic bridge and decided to go with a Thru Truss.

The new 155 ft long x 26 ft wide two-lane bridge is an all-bolted steel structure with a self-weathering finish and concrete deck. The bridge loading was rated HS25 and also carries utilities across the river.

Alternative 5

Features

Construct new two-span continuous steel beam bridge (80'-80') spans with concrete deck parallel to the existing truss Bridge. Retain existing truss bridge as-is to carry only pedestrian and bicycle traffic.

- New Bridge will carry two lanes, one lane each direction
- Meets minimum GS-4 criteria with two 10' lanes and 1' offsets to the railing on each side
- Designed for AASHTO LRFD HL-93 loading
- New Pier and Abutments
- Estimated Cost \$3.0M to \$4.0M

Alternative 5 – View 1

New Two-Span Continuous Steel Beam Bridge and Existing Truss Bridge for Pedestrians



Alternative 5 – View 2

New Two-Span Continuous Steel Beam Bridge and Existing Truss Bridge for Pedestrians

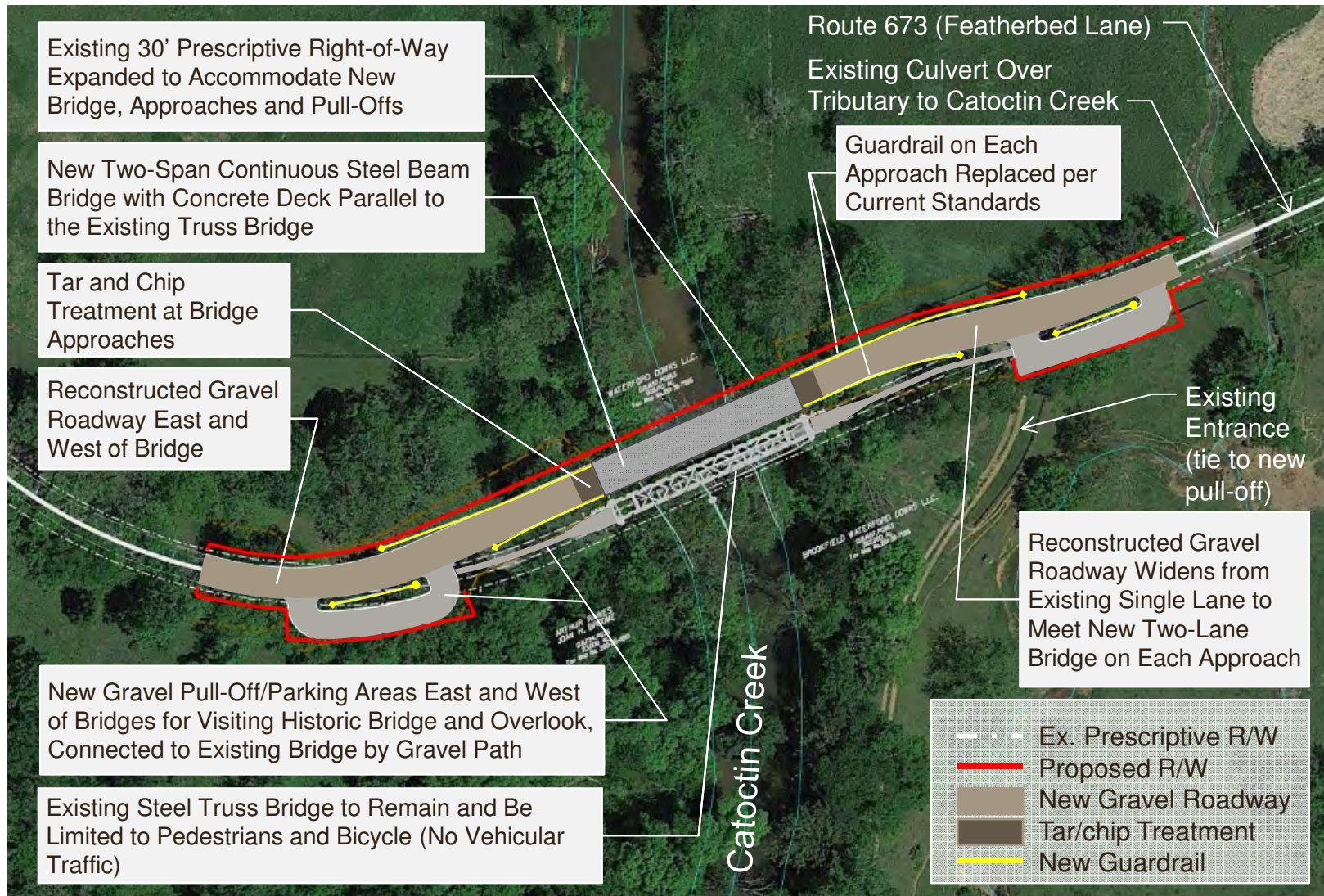


Alternative 5 – View 3

New Two-Span Continuous Steel Beam Bridge and Existing Truss Bridge for Pedestrians



Alternative 5 – Roadway Plan



Historical Truss Bridge Preservation Springbrook Road Truss over Linville Creek, Broadway, VA Single Span (136 ft) Thatcher through truss bridge, Built 1898



Before



After

Highlights:

Listed in the National Registry of Historic Places and in Virginia Landmarks Registry.

Built in 1898, the capacity was recently reduced to a 4 ton weight limit posting.

It was decided to close the bridge for safety reasons (non-redundant design can lead to instantaneous collapse, no emergency responder vehicle access).

In order to keep the bridge at the current location, the County decided to repurpose the one-lane Truss Bridge as a pedestrian bridge. A new parallel 2-lane bridge was constructed to carry traffic (4-spans, prestressed box beams with concrete overlay, 136 ft long x 32 ft wide).

The project maintains the old historic truss bridge in its current setting and provides a new bridge to meet current and future traffic needs.

Alternative 6

Features

Construct new single span steel pony truss bridge (160') with concrete deck

- Will carry two lanes, one lane each direction
- Meets minimum GS-4 criteria with two 10' lanes and 1' offsets to the railing on each side
- Designed for AASHTO LRFD HL-93 loading
- New Abutments
- Estimated Cost \$2.5M to \$3.5M

Alternative 6 – View 1

New Steel Pony Truss Bridge



Alternative 6 – View 2

New Steel Pony Truss Bridge



Alternative 6 – View 3

New Steel Pony Truss Bridge



Similar Historical Truss Bridge Replacement Clem Lowell Road Bridge, Carroll County, Georgia

Technical Description:

- Width: 28 -ft.
- Span: 130 -ft.
- Style: Cambridge Flat
- Finish: Weathering Steel
- Decking: Concrete

Installation Date: 2008

- Provided by U.S. Bridge



Highlights:

After providing maintenance and repair for the original 70 year old steel truss bridge for many years and lowering the load limit on the bridge to 3 tons, the County closed the bridge to traffic in 2008 after a heavy truck cracked members supporting the deck.

Recognizing the shortfalls of the now compromised bridge, Carroll County officials determined that instead of facing the prospect of continued repairs, it was best to replace the entire structure and install new abutments.

The County wanted the new structure to resemble the original structure. The new 130 ft long x 28 ft wide Pony Truss Bridge was selected as the structure that best replicated the old Clem Lowell Road Bridge while also providing the current load rating standards and structural integrity.

Alternative 7

Features

Replace existing Pratt thru truss bridge with a new through truss bridge similar in appearance

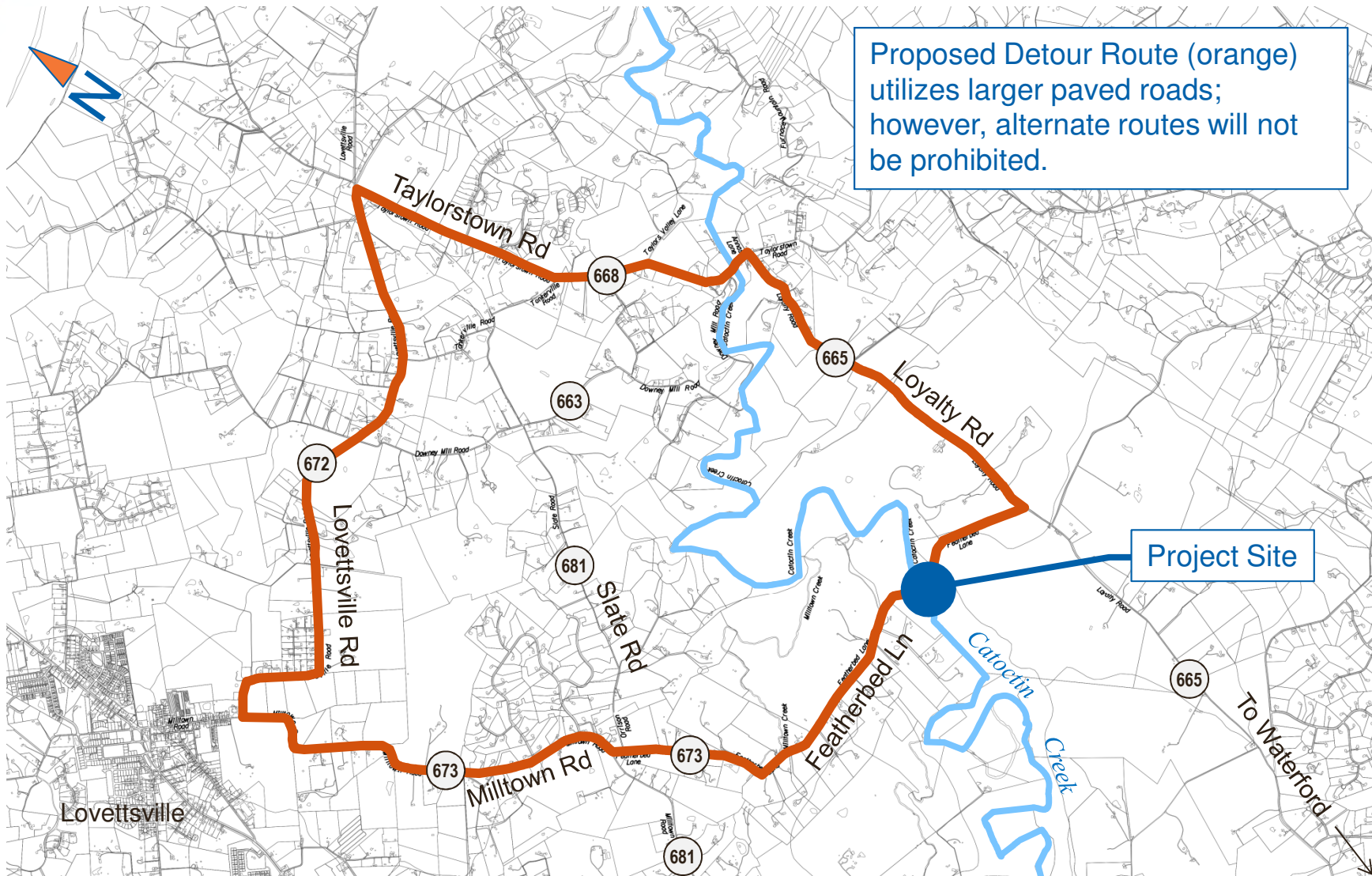
- Will carry one reversible lane
- Designed for AASHTO LRFD HL-93 loading
- Increases horizontal clearance between the bridge railings from 11'-2" to 14'
- Crash tested bridge safety railing
- Timber Glu-lam Deck
- **New Abutments**
- **Estimated Cost \$3.0M to \$4.0M**

John G. Lewis Memorial Bridge over Catoctin Creek

ALTERNATIVE 7 - New Single Lane Steel Thru Truss Bridge



Detour Route (All Alternatives)





QUESTIONS & COMMENTS

Vicente Valeza, P.E.
NOVA District Senior Structural Engineer
(703) 259–3256
Vicente.Valeza@vdot.virginia.gov

Link to the project webpage:

http://www.virginiadot.org/projects/northernvirginia/route_673_over_catoctin_creek.asp



John G. Lewis Memorial Bridge, (Featherbed Lane over Catoctin Creek)

September 16, 2015

Meeting of Interested Parties of the Section 106 Process

Agenda

1. Introductions – Gary Runco, PE (VDOT)
 - Please sign the attendance sheet
 - Meeting Purpose
 - Purpose and need for the project
2. Updates on the Cultural Resources Survey/Section 106 Process – Ray Ezell, RPA (VDOT)
3. Presentation of Revised Alternatives 1, 2 and 3, and additional 2A. And other previously presented alternatives – John Michels, PE (Consultant)
4. Existing Bridge Conditions and Responses to Structural Questions – Gary Runco, PE (VDOT)
5. Next steps
 - Effect Determination
 - MOA (Memorandum of Agreement)
6. Discussion
7. Future meeting(s)

VDOT Presenters

Gary Runco, P.E. - NoVA District Structures and Bridge Engineer

Raymond Ezell, RPA - District Archaeologist

Office: (540)654-1737, Raymond.Ezell@vdot.virginia.gov

John Michels, P.E. - Consultant Project Manager, Parsons Brinckerhoff

Project Contact

VDOT Project Manager- Vicente Valeza, Jr., PE

Office: (703)259-3256, Vicente.Valeza@vdot.virginia.gov

Project Website

www.virginiadot.org/projects/northernvirginia/route_673_over_catocotin_creek.asp

Schedule and Estimated Costs

Anticipated Schedule

Authorize Preliminary Engineering - Fall 2016

Draft Environmental Document - Spring 2017

Advertisement for Construction - September 2019

Start Construction - December 2019

Current Allocations

\$1.7M allocated in FY 2017-2020

JOHN G. LEWIS MEMORIAL BRIDGE (RTE 673 - FEATHERBED LANE)

OVER CATOCTIN CREEK

LOUDOUN COUNTY

STATE PROJECT NO. 0673-053-6051

Sign-In Sheet

Subject: Third Stakeholders Information Meeting for Section 106

Location: Old Waterford School, 40222 Fairfax Street, Waterford, VA 20197

Date & Time: September 16, 2015 – 11:00 am

Attendees:

	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
1.	<u>Craig Grea</u>		<u>703 864 3502</u>	<u>craig@cmackg.com</u>
2.	<u>John Simkins</u>	<u>FHWA</u>	<u>804-775-3347</u>	<u>John.Simkins@dot.gov</u>
3.	<u>MARC HOLMA</u>	<u>DHR</u>	<u>(804) 482-6090</u>	<u>marc.holma@dhr.virginia.gov</u>
4.	<u>Nancy Doane</u>	<u>Resident</u>	<u>540-882-3168</u>	<u>njsdoane@gmail.com</u>
5.	<u>VICENTE VALEZA</u>	<u>VDOT - PM</u>	<u>(703) 259-3200</u>	<u>vicente.valeza@dot.virginia.gov</u>
6.	<u>Alto Gibson</u>	<u>Resident</u>	<u>540 882 3205</u>	<u>_____</u>

JOHN G. LEWIS MEMORIAL BRIDGE (RTE 673 - FEATHERBED LANE)

OVER CATOCTIN CREEK

LOUDOUN COUNTY

STATE PROJECT NO. 0673-053-6051

Sign-In Sheet

Subject: Third Stakeholders Information Meeting for Section 106

Location: Old Waterford School, 40222 Fairfax Street, Waterford, VA 20197

Date & Time: September 16, 2015 – 11:00 am

Attendees:

	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
1.	Megan Compton	LC Board of Supervisors	703-771-5028	megan.fabling@loudoun.gov
2.	Lauren Murphy	LC Planning	703-777-0164	lauren.murphy@loudoun.gov
3.	Heidi Siebert	LC PLANNING	703-771-5115	heidi.siebert@loudoun.gov
4.	MARGARET GOOD	WATERFORD FDN	540-882-3018	mgood@waterfordfoundation.org
5.	Eleanor Adams	Catoctin Group Inc	540-882-3351	emc@adams.org
6.	Julie Bolthouse	PEC	571-442-4068	jbolthouse@pecva.org

JOHN G. LEWIS MEMORIAL BRIDGE (RTE 673 - FEATHERBED LANE)

OVER CATOCTIN CREEK

LOUDOUN COUNTY

STATE PROJECT NO. 0673-053-6051

Sign-In Sheet

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Date & Time: September 16, 2015 – 11:00 am

Attendees:

	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
1.	<u>HENRY PLASTER</u>	<u>MYAA/STA</u>	<u>540-534-6311</u>	<u>APLA739142@aol.com</u>
2.	<u>Martha Clagett</u>	<u>resident</u>	<u>(540)882-3068</u>	<u>csjetstone@yahoo.com</u>
3.	<u>Steve Varcoe</u>	<u>VDOT</u>	<u>703 259 2819</u>	<u>Steve.Varcoe@VDOT.Virginia.gov</u>
4.	<u>Anissa Brown</u>	<u>VDOT - ENV</u>	<u>703 259 3358</u>	<u>anissa.brown@vdot.virginia.gov</u>
5.	<u>John Caron</u>	<u>resident</u>	<u>540-882-3168</u>	<u>john.a.caron@gmail.com</u>
6.	<u>DAVID NELSON</u>	<u>Res. Dev</u>	<u>5408224639</u>	<u>DANelson2009@Gmail.com</u>

JOHN G. LEWIS MEMORIAL BRIDGE (RTE 673 - FEATHERBED LANE)

OVER CATOCTIN CREEK

LOUDOUN COUNTY

STATE PROJECT NO. 0673-053-6051

Sign-In Sheet

Subject: Third Stakeholders Information Meeting for Section 106

Location: Old Waterford School, 40222 Fairfax Street, Waterford, VA 20197

Date & Time: September 16, 2015 – 11:00 am

Attendees:

	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
1.	Phil Ehrenkranz		540 882 3524	pokerbloke@taylorstown.net
2.	DAVID WARD		540 822-5092	dward@earthwardconsulting.com
3.	Mitch Diamond			
4.	MAX CROOK		717 930-1854	MAXIMUSCROOK@HOTMAIL.COM
5.	JENN CROOK		703-944-3709	JENNIFER LYNN CROOK@YAHOO.COM
6.	RAY EZELL	VDOT	540 907 5994	raymond.ezell@vdot.virginia.gov

	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
	Henderson			
7.	Kitty (On Phone)			
8.	Nathan (On Phone)			
9.	Sunil Taori	VDOT	703- 259-3258	Sunil.Taori@vdot.virginia.gov
10.	Gary Runco	" "		
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	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
7.	<u>JOHN MICHELS</u>	<u>PARSONS BRINCKERHOFF</u>	<u>703.742.5843</u>	<u>MICHELS@PBWORLD.COM</u>
8.	<u>DANIEL DAVIES</u>	<u>Del. Dave LaRock</u>	<u>540-751-8364</u>	<u>deldlarock@house.virginia.gov</u>
9.	_____	_____	_____	_____
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	<u>Name</u>	<u>Office/Company</u>	<u>Phone</u>	<u>Email</u>
7.	<u>Hari Amidala</u>	<u>Parsons Brinckerhoff</u>	<u>703-345-8123</u>	<u>AAMIDALA@pbworld.com</u>
8.	_____	_____	_____	_____
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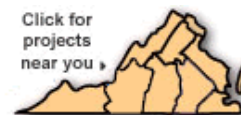
John G. Lewis Memorial Bridge over Catoctin Creek

Route 673 - Featherbed Lane over Catoctin Creek in Loudoun County

About the Project

The purpose of this project is to develop conceptual bridge alternatives to address the existing historic truss bridge. Six (6) alternatives are being evaluated including the restoration of the existing truss bridge to the previous 15 ton vehicle capacity; or repurposing the bridge as a pedestrian bridge and providing a parallel vehicular bridge; or replacing the bridge with a new truss vehicular bridge or bridge with a truss facade.

John G. Lewis Memorial Bridge (Route 673 - Featherbed Lane) over Catoctin Creek is a single span steel pin connected Pratt through truss with a timber deck and asphalt overlay, supported on stringers and floor beams. It is 157 feet long, has an overall width of about 14 feet and clear roadway width of 11.16 feet. It carries one alternating traffic lane. The bridge was originally erected in 1889 on the Leesburg & Alexandria Turnpike (Route 7) over Goose Creek, to replace a bridge destroyed by flooding. In 1932 the bridge was dismantled and moved to its current location. The bridge was posted 15 tons in 2004 and reduced to 3 tons in 2013 due to insufficient capacity. The existing bridge has an average traffic count of 110 vehicles per day with daily truck traffic less than 3



Area Maps

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Six-Year Improvement Program database

trucks.

The bridge was listed in the National Register of Historic Places on June 25, 1974.

Schedule & Costs

Anticipated Schedule

Authorize Preliminary Engineering – Fall 2016
Draft Environmental Document – Spring 2017
Advertisement for Construction – September 2019
Start Construction – December 2019

Estimated Costs

Preliminary Engineering - \$800,000
Right-of-Way - \$200,000
Construction - \$3 million
Total - \$4 million

Resources

Stakeholders Meeting (September 16, 2015)

[Existing Conditions presentation \(http://www.virginiadot.org/Route_673_Featherbed_Lane_Bridge_final_9-16-15_Runco.pdf\)](http://www.virginiadot.org/Route_673_Featherbed_Lane_Bridge_final_9-16-15_Runco.pdf)

[Evaluation of Strengthening/Replacement Alternatives presentation \(http://www.virginiadot.org/Rte_673_Truss_Alternatives_-_Public_Meeting_9.16.pdf\)](http://www.virginiadot.org/Rte_673_Truss_Alternatives_-_Public_Meeting_9.16.pdf)

[Agenda \(http://www.virginiadot.org/Sept_16_Meeting_Agenda.pdf\)](http://www.virginiadot.org/Sept_16_Meeting_Agenda.pdf)

[Sign-in sheet \(http://www.virginiadot.org/Sign_In_Sheet_9.16.2015.pdf\)](http://www.virginiadot.org/Sign_In_Sheet_9.16.2015.pdf)

Stakeholders Meeting (July 23, 2015)

[Existing Conditions presentation \(http://www.virginiadot.org/Route_673_Featherbed_Lane_Bridge_final_7-23-15_version.pdf\)](http://www.virginiadot.org/Route_673_Featherbed_Lane_Bridge_final_7-23-15_version.pdf)

[Alternative 7 presentation \(http://www.virginiadot.org/Alternative_7_Presentation_and_Sample_Project_7.23.2015.pdf\)](http://www.virginiadot.org/Alternative_7_Presentation_and_Sample_Project_7.23.2015.pdf)

[Handout \(http://www.virginiadot.org/July_23_2015_Stakeholders_Meeting_Handout.pdf\)](http://www.virginiadot.org/July_23_2015_Stakeholders_Meeting_Handout.pdf)

[Sign-in sheet \(http://www.virginiadot.org/Sign_In_Sheet_7.23.2015.pdf\)](http://www.virginiadot.org/Sign_In_Sheet_7.23.2015.pdf)

[Section 106 presentation \(http://www.virginiadot.org/Featherbed_Ln_Sect106presentation_Jul23.pdf\)](http://www.virginiadot.org/Featherbed_Ln_Sect106presentation_Jul23.pdf)

Stakeholders Meeting (March 26, 2015)

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[Existing Conditions presentation \(http://www.virginiadot.org/Rte_673_Featherbed_Lane_Bridge_Existing_Conditions_3.26.pdf\)](http://www.virginiadot.org/Rte_673_Featherbed_Lane_Bridge_Existing_Conditions_3.26.pdf)

Project Photos



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Project at a Glance

Begin Date
December 2019

Cost
\$4 million

Locality
Loudoun

District
Northern Virginia

Contact
[Vicente Valeza, Jr., P.E.](mailto:Vicente.Valeza@vdot.virginia.gov)
(mailto:Vicente.Valeza@vdot.virginia.gov)Senior
Structural Engineer
703-259-3256

<http://syip.virginiadot.org/Pages/allProjects.aspx>

[Evaluation of Strengthening/Replacement Alternatives presentation \(http://www.virginiadot.org/Rte_673_Featherbed_Lane_Truss_Alternatives_3.26.pdf\)](http://www.virginiadot.org/Rte_673_Featherbed_Lane_Truss_Alternatives_3.26.pdf)

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[Section 106 presentation \(http://www.virginiadot.org/Featherbed_Ln_bridge_Sect106_presentation_Mar26.pdf\)](http://www.virginiadot.org/Featherbed_Ln_bridge_Sect106_presentation_Mar26.pdf)

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Transparency in Procurement
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Project Numbers

State: 0673-053-6051
UPC: 105898

Page last modified: Sept. 18, 2015

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